

CITY OF CODY
PLANNING, ZONING, AND ADJUSTMENT BOARD
TUESDAY SEPTEMBER 10, 2019
CITY HALL COUNCIL CHAMBERS @ 12:00 NOON

1. Call meeting to order.
2. Roll Call, excused members
2. Pledge of Allegiance
3. Approval of Agenda
4. Approval of Minutes of the September 3, 2019 regular meeting.
5. BUSINESS:
 - A. Review a site plan for the Hampton Inn, located at 8 Southfork Road.
 - B. Review the amended preliminary plat and final plat for the Park County School District #6 Beacon Hill minor subdivision. Property is located west of Beacon Hill and north of Sheridan Avenue
 - C. Amend the approved sign for the Cody Country Art League, located at 836 Sheridan Avenue.
6. P & Z Board matters (announcements, comments, etc.)
7. Council Update
8. Staff Items
11. Adjourn

The public is invited to attend all Planning, Zoning and Adjustment Board meetings. If you need special accommodations to participate in the meeting, please call the City office at (307) 527-7511 at least 24 hours in advance of the meeting.

City of Cody
Planning, Zoning, and Adjustment
Board Tuesday, September 3, 2019

A meeting of the City of Cody Planning, Zoning and Adjustment Board was held in the Council Chambers of City Hall in Cody, Wyoming on Tuesday, September 3, 2019 at 12:00 pm.

Present: Chairman Kayl Mitchell; Erynne Selk; Buzzy Hassrick; Richard Jones; Sandi Fisher; Stan Wolz; City Attorney Scott Kolpitcke; City Planner Todd Stowell; Council Liaison Glenn Nielson; Administrative Coordinator Bernie Butler.

Absent: Klay Nelson

Kayl Mitchell called the meeting to order at 12:00 pm, followed by the pledge of allegiance.

Buzzy Hassrick made a motion, seconded by Sandi Fisher, to approve the agenda for September 3, 2019. Vote on the motion was unanimous, motion carried.

Stan Wolz made a motion, seconded by Buzzy Hassrick to approve the minutes from the July 30th, 2019 meeting. Vote on the motion was unanimous, motion carried.

NEW BUSINESS:

A. Todd Stowell presented an architectural district sign review for Nordberg & Lindauer, located at 1241 Sheridan Avenue.

Buzzy Hassrick made a motion, seconded by Erynne Selk to approve the architectural and sign plan for Nordberg & Lindauer. Vote on the motion was unanimous, motion passed.

B. Todd Stowell presented a site plan review for Quality Asphalt shop and office building, located at 172 Blackburn Street.

Richard Jones made a motion, seconded by Buzzy Hassrick to approve the site plan for 172 Blackburn Street, with staff recommendations 1-5 and add a recommendation #6 for the applicant to check with DEQ to see if a hazardous waste permit is needed for their business. Vote on the motion was unanimous, motion carried.

C. Todd Stowell reviewed the final plat for the Overfield 5-lot infill minor subdivision located on 31st Street. Todd went over the status of the preliminary plat conditions.

Erynne Selk made a motion, seconded by Richard Jones to recommend to the City Council the approval of the final plat for the Overfield minor subdivision, with conditions 1-7 in the staff report. Vote on the motion was unanimous, motion carried.

The next meeting will be held on September 10, 2019. Todd Stowell asked the Board member to read the letter on cargo storage units and discuss this at a future meeting.

Erynne Selk made a motion, seconded by Sandi Fisher to adjourn the meeting. Vote on the motion was unanimous, motion carried.

There being no further business to come before the Board, Chairman Kayl Mitchell adjourned the meeting at 12:34.

Bernie Butler, Administrative Coordinator

The 4-story building would have a footprint of 14,984 square feet, and 58,160 square feet of total floor area. Civil plans, and selected architectural, electrical, and other building plans are attached. While additional plan sheets have been provided to staff, the attached sheets show the main components and general layout of the items reviewed by the Planning and Zoning Board. If any Board members wish to see additional details, the full plans are available.



Existing Conditions:



REVIEW CRITERIA:

Section 10-10C-5 of the zoning regulations states:

All structures within the district shall be architecturally compatible. Architectural and landscaping plans shall be submitted to the planning and zoning commission for

approval. Architectural and landscaping details shall be maintained as shown by the approved plans.

Section 9-2-3 is as follows:

Before the issuance of any permit under the international building code for commercial buildings situated within the city, the applicant, property owner and occupant shall meet with the planning, zoning and adjustment board to review the application and plans insofar as they pertain to the exterior of a commercial building and site plan conditions. The issuance of a permit shall be conditioned upon the applicant receiving an affirmative vote of a majority of the planning, zoning and adjustment board members in attendance at said meeting.

Applicable standards of the zoning district are also reviewed.

STAFF COMMENTS:

The property is located within the Open Business/Light Industrial (D-3) zoning district, which permits hotels. The surrounding area is as follows:

<i>DIRECTION</i>	<i>EXISTING USE</i>	<i>ZONING</i>
North/West	Property used for Game & Fish check station, across Southfork Road (Highway 291).	D-3
East	Vacant church buildings.	D-3
South	Vacant hillside--Valley View subdivision on top of hillside above.	D-3

Architecture:

Based on comments received as part of the January 2019 review, the applicant changed the exterior color scheme and modified some of the exterior materials. A rendering of the current proposal is shown below.



The 4-story building is proposed with a flat roof surrounded by parapet walls to help screen roof-mounted equipment and provide variation to the roofline. Variation in the exterior walls is provided through the use of different colors and thicknesses of the EIFS finish to create a faux recessed window look to the front central rooms, by stepping wall sections forward/back, and further visually breaking up the 257'6" long and 57'6" tall building through the use of different exterior materials and colors. The lower portions of the exterior walls would have a manufactured stone finish. The brown areas in the central portion of the front of the building have a wood-grained panel look. The rest of the exterior would have an EIFS (stucco look) finish in the colors indicated.

- Material Legends**
- P1 EIFS - BENJAMIN MOORE
COLOR: DESERT LIGHT - 1004
 - P2 EIFS - BENJAMIN MOORE
COLOR: MEDITATION - AF-395
 - P3 EIFS - BENJAMIN MOORE
COLOR: TAUPE FEDORA - CSP-260
 - P4 EIFS - BENJAMIN MOORE
COLOR: NORTH CREEK - 1001
 - P5 EIFS - BENJAMIN MOORE
COLOR: BOREAL FOREST - AF-480
 - WOOD LOOK KAIN SCREENING
 - STONEWOOD - STANDARD
COLOR: MAMBO - 2588-CB
 - STONE
 - INSULSTONE
COLOR: CHARCOAL MLP
 - WINDOW FRAMES



SOUTHWEST ELEVATION ②



NORTHWEST ELEVATION ①

DEVELOPMENT DESCRIPTION
 NEW CONSTRUCTION:
 4 STORY
 58,100 SF - 92 KEY HOTEL

Sample of exterior materials—wood-look panels and manufactured stone:



MLP (MTN.) Panels		
Flats:	Height	Long
	6.00"	22.00"
Comers:	Long Leg	Short Leg
	8.5"	3.75"

The Board will need to determine if the architectural features of the building are acceptable. Staff is pleased to see the variety of color and quality materials, the new color scheme, and the other architectural components mentioned.

Landscaping:

The landscaping is of a quality design and would cover effectively everywhere available outside of the parking spaces, drive aisles, building pads, and storm water swale. They even have some landscaping extending into the WYDOT right-of-way along Southfork Road. The landscaping plan is found on Sheet 6. It is not fully updated to reflect the current plan, in that the rock "creek" along the highway right-of-way that was originally planned to convey storm water has been replaced with buried pipe. The plantings in that area are still proposed, but the ground cover is not identified.

The landscape designer is the same one that the cemetery district has used on their recent projects (Steiner Thuesen), which landscaping has been done with professional quality. The plants selected for this project appear suitable for the area and contain a variety of trees, shrubs, perennials, and ornamental grasses. A rock mulch groundcover (e.g. crushed limestone or similar) would be the primary groundcover.

A note is included on the landscape plan that the landscape areas may need soil fill due to the amount of cobble present. Irrigation will be with city water. Plans indicate a 1½" irrigation service with backflow prevention and conduit under the parking areas to distribute the irrigation water to all areas with vegetation.

The property is within the Entry Corridor Overlay zone (refer to City Code 10-17), which specifies minimum standards for landscaping. The proposed landscaping plan exceeds the standards of the entry corridor overlay ordinance. The landscaping component of this project is a good example for others.

The following adjustments to the landscaping plan are needed to avoid conflicts with proposed lighting fixtures or turning movements of large vehicles:

- a) Adjust the two trees at the entrance so that their center is at least 10 feet from the curbing on the access aisle.
- b) Relocate or remove the proposed tree at the southwest end of the island with the dumpsters.
- c) Relocate the second tree from the north, on the east side of the lot, so that the location does not conflict with the lighting fixture proposed for the same location.

Access:

Access to the project is from Highway 291, which is managed by WYDOT. The applicant was required by WYDOT to provide updates to the traffic study that analyzed the traffic impacts of this project on the highway system. Ultimately, the applicant obtained new traffic counts during the summertime, reanalyzed the impacts, and received conceptual approval for their access plan.

The access plan is indicated in concept on the site plans—it involves relocating the existing approach about 160 feet to the south (100 feet further than indicated in the January review) and converting the inside, southbound lane to a turn lane into the site. Curb, gutter and sidewalk will be extended from the north, southward into the property and the area between existing asphalt and the new curb will be paved.

Section 10-16-8(D) of the City code is as follows:

D. Entrances/Exits: ...

... For access to state highways, access management techniques shall be determined through consultation and permitting with WYDOT prior to city approval of the parking plan. (See also WYDOT "Access Manual", 2005 or current edition.)

Staff believes that the WYDOT conceptual approval is sufficient to proceed with the zoning review, as the access location and highway improvements have been specified. The engineering level detail will be provided for WYDOT review and approval.

The plans call for use of the existing entrance during construction. Public works prefers that the new entrance and highway striping be installed as soon in the construction process as possible, due to the amount of construction workers and associated vehicles that are expected. WYDOT does not appear to be as concerned with the timing. That discussion is still in progress. Planning staff believes that the timing discussion can occur between the owner and those agencies.

The relocation of the approach moves it beyond the end of the existing access easement for the lot to the east (Parcel A). The property owner will need to extend the access easement to encompass the new approach location. It does not need to be as wide as the existing easement. The easement should be created and recorded prior to issuance of the building permit. The applicant is coordinating the revised easement with the property owner to the east (Parcel A).

It is noted that the WYDOT conceptual access approval requires the owner of the lot to the east to complete a future traffic study and associated WYDOT analysis and permitting before they can begin using the approach. The current approval is limited to the Hampton Inn project only. Speaking with WYDOT, they recognize that the lot to the east is expected to generate some traffic, they just don't want to issue an open-ended permit at this time in case a large traffic generator is proposed for that property that would require additional highway improvements.

As Public Works will have some maintenance responsibility for the new highway improvements, they request the opportunity to review and comment on the engineering plans for those improvements.

Parking

Dimensionally, the parking spaces and aisles meet the minimum standards. The parking ordinance recommends that parking for hotels be provided at a ratio of 1.1 spaces per rentable room plus 1 space per hotel vehicle, and 0.75 spaces per employee on the maximum shift. The application notes that this would be 107 spaces. The plan has 112 standard spaces shown, six of which are ADA spaces. An additional six motorcycle spaces are also provided.

Analysis has been provided that fire trucks, emergency vehicles, and garbage trucks can maneuver in all areas necessary through the parking lot without the need for backing to make a corner. The only turning movement that cannot be made by those vehicles (without backing) is the turn north of the island at the front of the building.

Exterior Lighting

Lighting details are provided on Sheets E1.0, E1.1, and the photometric plan (shows illumination levels across site in foot candles).

The lighting plan utilizes full-cutoff and shielded fixtures in all cases except the ground mounted flood lights (S2) designed to illuminate the front (north side) of the building, and the low-illumination wall fixtures that are shielded with translucent glass/plastic (P1). All of the planned fixtures are indicated below.

C4: (Recessed can lights under portico) Not shown.

L8:



P1:



S:



S2:



S3:



S4 (T4M), S5 (T3M), and S6 (T5M):



The rope lighting (S above) is located at the ends of the building in a cove that wraps around the green colored area. It will create a glow effect, but is not directly visible.

Fixture S2 is the spotlight fixture designed to illuminate the front of the building. Staff has some concern that the spotlights towards the west end of the building and on the front corners of the portico could cause glare issues for vehicles traveling north on the Southfork Hill. Careful coordination with the landscaping and/or shielding should be able to avoid the issue. Also, the fixtures should be carefully aimed, so as to not spill excessive light above the walls of the building.

The parking lot fixtures are proposed to be mounted on 25' poles. With a standard 3-foot base, they are in line with heights authorized for other large commercial projects.

The overall lighting plan clearly illuminates the project, but does not result in excessive light spillage beyond the property lines, other than in the immediate area along the east side. As the spillover is fairly minimal and that area is also commercial, it is not of concern to staff.

The International Dark Sky Association, an organization that attempts to educate the public about lighting effects and ways to minimize light pollution, indicates that parking lots with high levels of activity (fast food, regional shopping centers, etc.) should not exceed an average of 3.6 foot candles. For parking lots with medium levels of activity (hospitals, residential multi-family complexes, etc.) their recommendation is 2.4 foot candles. The photometric map for this project indicates average illumination levels of 2.8 foot candles in the parking lot. Within the landscape area, illumination averages 4.8 foot candles.

The only modification staff has identified that could be considered to further minimize the lighting impacts from the project is to use a "warmer" light. The International Dark-Sky Association notes that light with a temperature of 4,000 kelvin or more has excessive blue light, and recommends the use of light fixtures that produce a warmer light of 3,000 kelvin or less (publication attached). The Hampton Inn plans call for use of exterior lights of 4,000 kelvin. Blue light from the proposed LED light fixtures, like other sources of blue light (computer screens, electronic devices) can affect sleep and health. If the developer wishes to claim full compliance with Dark Sky standards, whether for marketing or other purposes, the change would be needed. Staff is not to the point of recommending a change in the lighting plan, but felt sharing the information is appropriate.

Neighborhood Compatibility, Setbacks and Buffers, and Height Requirements

There is no immediately adjacent residential zoning that would trigger buffer or setback requirements. There are no specified zoning setbacks or building height limits in this zone. The building meets applicable setbacks under the building code.

Storm Water Plan:

An analysis of the storm water was provided by the applicant's engineer. The storm water will be managed as noted in the grading and drainage plan (Sheets 3 and 4) and the updated drainage report (revised Aug. 22, 2019). The roof drainage, patio drainage, and much of the parking area will be collected through a series of inlet grates, curb cuts, and 15-inch pipe throughout the site and piped to an open swale at the southwest end of the property. The swale is sized much, much larger than the minimum required, which is good because there is the possibility that it will also intercept some drainage coming off of the highway.

The swale will also be used to discharge water from the swimming pool and hot tub when maintenance necessitates.

From the landscaping plan, it appears that the sides of the drainage swale will be lined with a native grass mix (non-irrigated). To minimize the erosion potential there are some stabilization mats proposed. Staff recommends also that the slope be modified to roughly a 3:1 slope to further minimize erosion potential. As one side is effectively 4:1 and one side 2:1 now, it is simply a matter of using the average 3:1 slope on both sides. Also, the outlets will need grates to prevent animal entry, and rock to dissipate energy from the flowing water.

The relatively steep slope along the east side of the parking lot is expected to be a challenge for the landscapers and landscaping maintenance crew. Staff recommends a maximum 3:1 slope (33%) and preferably even less in the landscape strip along the east side. The simple solution would appear to receive permission from the adjacent property owner to extend the fill slightly onto the adjacent property.

Public Works notes that the grading plan does not include the typical spot elevation detail necessary for construction of a facility this complex. The applicant's engineer claims that what is provided is adequate, and that they will be able to address remaining grading detail in the field during construction. The Planning and Zoning application form does not specifically require the level of "spot elevation" level of detail. However, if it is done, staff requests the opportunity to review.

Snow Storage

Snow storage is not shown. Snow piles in some of the parking spaces near the perimeter inlets or curb cuts would be acceptable, as maximum capacity of the hotel in the winter is unlikely, and the piles would be in an area that they could melt directly into the storm water system.

Utility Services

The planned utilities are shown on sheet 5. The applicant is proposing that the sewer and water line extensions be private lines, which is acceptable to the City. Construction plans for the water and sewer extensions have been permitted by DEQ and meet City

requirements.

The electrical work has been coordinated with the City, and estimates for materials provided to the applicant. An easement for the electrical extension will need to be granted before the line is activated.

Natural gas and telecommunication services are to be coordinated directly with those providers.

Utility fees have been calculated and the numbers provided to the applicant.

Signage

A sign application is not included. The exterior signage will require review and permitting at staff level. It is noted that the monument sign location shown on Sheet E1.0 is directly over the proposed power line. Public Works does not allow signage in their easements, so the sign will need to be shifted. WYDOT doesn't allow sign supports in their right-of-way either.

Hydrants/Fire Line

The water line extension provides the required fire hydrant. The building will have fire sprinklers throughout. Water pressure and flow is good in the main. The design of the fire sprinkler system design, with flow rates and pressures will be reviewed with the building permit.

Grading in Highway Right-of-way

The grading plan has been modified so that there is no grading in the highway right-of-way other than the approach. This change maintains the ability for a future trail along the south edge of the highway right-of-way, as shown on the master trail plan. Installation of the trail segment is not expected of this development.

Garbage

The dumpster enclosure area is in the island near the southwest end of the parking lot. It is designed for a capacity of three dumpsters. They will need to be rolled out of the enclosure for pick up.

Fencing

The dumpster enclosure fence/wall, is to be constructed of split-face masonry block and is proposed to be ten feet tall, with a 9-foot chain link gate with privacy slats. Fences/walls over seven feet tall require special permission from the Planning and Zoning Board. As the fence is well internal to the site and matches the height of the connected storage building, the added height is not expected to affect any neighboring property owners. The additional height should give additional protection from the wind.

The perimeter wall around the outdoor amenity area (BBQ grill, firepit, hot tub, lounge

chairs) is proposed at 6'8".

ATTACHMENTS:

Application materials.

ALTERNATIVES:

Approve or deny the site plan, with or without changes.

RECOMMENDATION:

The updated plans are relatively clean and have incorporated almost all comments and conditions identified in the January 2019 review. Staff recommends approval of the project subject to the following conditions:

1. Provide an updated access easement from the approved new approach to the lot to the east (Parcel A) as soon as reasonably possible, but no later than 30 days from the date of site plan approval.
2. Provide a copy of the outstanding WYDOT permits, including the permits associated with the access modifications and utility connections in the right-of-way prior to construction of each. Also update the landscaping permit. (Expires before it will be completed, and it does not reflect the latest plan.)
3. All highway and access improvements must be completed and accepted by WYDOT prior to occupancy of the hotel, or earlier if specified by WYDOT.
4. Provide Public Works the opportunity to review and comment on the construction plans for the access and highway modifications.
5. Provide updated plumbing plans (Sheet P1.1A) reflecting the new route for the roof runoff (west end of building instead of east end).
6. Modify the plans for the slopes of the drainage basin and the landscape strip along the east property line to have maximum 3:1 slopes. Obtain written permission from the adjacent property owner if fill is to extend onto that property.
7. Provide grates and energy dissipation/erosion control structures (i.e. rip rap) at the ends of the storm water pipes.
8. Provide a utility easement for the electrical and telecommunication extensions, as shown on the plans, prior to activation of the utilities. Any freestanding sign will need to be relocated outside of the City electric utility easement—modify the plan (Sheet E1.0) accordingly.
9. Provide "as-constructed" plans for the sewer and water mains.
10. No lighting shall cause glare impacts to neighboring properties or drivers on the highways. Utilize landscaping and shielding as needed.
11. Make the following adjustments to the landscaping plan to avoid conflicts with proposed lighting fixtures or turning movements of large vehicles:
 - a. Adjust the two trees at the entrance so that their center is at least 10 feet from the curbing on the access aisle.
 - b. Relocate or remove the proposed tree at the southwest end of the island with the dumpsters.

- c. Relocate the second tree from the north, on the east side of the lot, so that the location does not conflict with the lighting fixture proposed for the same location.
- 12. Clarification—the fire lane is required as shown on the civil site plans—not the shorter length shown on the architectural site plan (A12.1).
- 13. The fire marshal has been occupied with fires, and I have been unable to confirm if he is requiring painted curbs and/or “No Parking/Fire Lane” signs in addition to the two signs shown. Paint and/or mark curbs as required by the fire marshal.
- 14. Provide a rolled curb for the fire lane, and indicate its construction detail on plans.
- 15. Applicable city utility fees are to be paid with the building permit fee.
- 16. The project must otherwise comply with the approved site plan, architectural elevations, and applicable building, fire, and electrical codes.
- 17. A building permit must be obtained within three years or this authorization will expire.



central design
group

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HAMPTON INN & SUITES
CODY, WYOMING

RENDERING IS FOR REPRESENTATION PURPOSE ONLY

ARCHITECTURE | PLANNING | INTERIOR DESIGN | PROCUREMENT
3200 Penn Terrace, Suite 100 | Columbia, Missouri 65202 | 573.441.9391



central design group

Hampton Inn & Suites PROPOSED ELEVATIONS

Cody, Wyoming



NORTHWEST ELEVATION ①

DEVELOPEMENT DESCRIPTION

NEW CONSTRUCTION:
4 STORY
58,160 SF - 92 KEY HOTEL



Project Site Location

Material Legends

- P1 EFIS - BENJAMIN MOORE
COLOR: DESERT LIGHT - 1004
- P2 EFIS - BENJAMIN MOORE
COLOR: MEDITATION - AF-395
- P3 EFIS - BENJAMIN MOORE
COLOR: TAUPE FEDORA - CSP-260
- P4 EFIS - BENJAMIN MOORE
COLOR: NORTH CREEK - 1001
- P5 EFIS - BENJAMIN MOORE
COLOR: BOREAL FOREST - AF-480
- WOOD LOOK RAIN SCREENING
STONEWOOD - STANDARD -
COLOR: MAMBO - 2588-CB
- STONE
INSULSTONE
COLOR: CHARCOAL MLP
- WINDOW FRAMES



SOUTHWEST ELEVATION ②

ARCHITECTURE | PLANNING | INTERIOR DESIGN | PROCUREMENT

3200 Penn Terrace, Suite 100 | Columbia, Missouri 65202 | 573.441.9391

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August 30, 2019

Todd Stowell, AICP
City Planner
City of Cody
1338 Rumsey Avenue
Cody, WY 82414

Subject: Hampton Inn & Suites Site Development Outstanding Items

Dear Todd:

As you know, we have been working with WYDOT since the first submittal of the development packet for the Hampton Inn to secure the approach permit for the property. Fortunately, it appears we have had success and will indeed receive the necessary permit. Therefore, we are requesting the proposal be re-visited with the hope of obtaining approval from the Planning, Zoning and Adjustment Board at the September 10 meeting. As part of the original submittal, numerous concerns were listed in the Staff Report that required attention. Below are the items and our responses.

Outstanding Items/Changes Needed:

1. Provide WYDOT's review of the proposed access and any modifications required thereby.

WYDOT has agreed in principle to our current proposal and we are working on the details required for the approval. We believe you were copied on the letter but if you need us to provide a hard copy, please let us know.

2. Provide permits from WYDOT for all work within the right-of-way (utilities, any additional grading, new approach, removal of approach, curb and gutter, sidewalk).

Permits have been submitted via email. The license to install water and sewer across the WYDOT right-of-way is forthcoming. We have been informed verbally the license will be approved.

3. Preserve a minimum 10-foot wide level strip in the WYDOT right-of-way for a future pathway, from the approach to the south property line.

This concern was raised due to our previously indicating significant grading work proposed for construction of the storm water percolation area. We are not proposing any grading work other than for the approach within the right-of-way, so the existing condition will remain.



4. Provide written documentation from the owner of the lot to the east (Parcel A) that the approach and other improvements in their access easement are acceptable.

An access easement between property owners was put in place as part of the purchase of the property. A revised easement is being prepared due to the shift of the approach and will be provided when it has been fully executed and recorded.

5. Provide information on the pipe size, grade, and elevation of the roof drain pipe to the infiltration basin.

The runoff will now be directed to a storm sewer system consisting of inlets and pipe. Details for all sections have been added to the plan set.

6. Add details for the infiltration basin, including: lining with cobble or grass, erosion control for pipe outlets, any modifications to slope on the west side due to the required 10' level strip.

Details have been added, and as mentioned above no modifications are proposed for the right-of-way.

7. Provide a detail for the curb cuts.

Done.

8. Clearly show a drainage ditch or method of conveying storm water along the south property line to the infiltration basin.

The runoff will now be conveyed in pipe as depicted in the storm sewer details.

9. Any details or considerations that the Public Works director determines is lacking in the storm water/drainage plan.

Will provide if necessary.

10. Add curbing to extend to the northeast, tying into the existing curb. Also add sidewalk from the east property line extended north, along the back of the new curbing and around the radius of the approach into the site (design to be reviewed).

With the elimination of the existing approach, the existing curb and gutter will be removed around the radius back to a point along the merge lane. The developer is also planning to extend sidewalk from the new approach to the end of the existing sidewalk east of the property. These plans are shown generally in this plan set but will be shown in detail on the plan set being prepared for WYDOT's review.

11. Update the plumbing plan (Sheet P1.1B) to reflect the change from a 3" to 4" domestic water service. Provide an estimated maximum flow rate to verify the proper sizing of the water meter, prior to issuance of a building permit.

Done.

12. Provide information on your research as to whether there is any old septic tank or drain field to address.

The Developer inquired with the previous owner and they are unaware any septic systems. Anything encountered during construction will be taken care of appropriately.

13. Provide details on the elevation/slopes of the dumpster enclosure floor and adjacent pad (to verify a person can roll out the dumpsters for collection, but they won't roll on their own).

Per the architect's details, the floor of the enclosure will slope six inches from the back to the front. The adjacent pad will be minimally sloped in order to drain.

14. Update Sheet E1.0 and E1.1 to detail the correct transformer and utility box locations, reflect the modifications to the parking lot in the northeast area, and remove the monument sign from the WYDOT right-of-way (unless permitted otherwise by WYDOT).

Done. The monument sign has been moved to the hotel property. The sign manufacturer will submit a sign application at a later date.

15. Identify the height of the parking lot light poles on the plans (25').

The luminaire schedule on sheet E4.1 contains a note for the light poles to be 25-ft on 30-in concrete bases.

16. Add a note on the site plan about installation of the irrigation distribution conduits and electrical conduits prior to construction of the parking lot facilities.

Irrigation conduits have been added to the landscape plan. Power cable will be installed entirely within conduit.

17. Provide the updated architectural/utility plans for review (coordinate with civil plans).

Done.

Recommended Conditions of Approval:

1. Unless otherwise specified by WYDOT, shift the approach so that it lines up with the drive aisle to the front of the hotel (approximately 20 feet), to avoid the offsets at the "intersection".

With the final approach location, this has been achieved and the driveway lines up with the west side of the entrance to the porte cochere.

2. Extend the access easement to include the new approach, prior to issuance of a building permit.

In progress. See Item #4 above.

3. No lighting shall cause glare impacts to neighboring properties or drivers on the highways. Utilize landscaping and shielding as needed.

Noted. Shielding will be installed.

4. Applicable city utility fees are to be paid with the building permit fee.

Noted.

5. Obtain the necessary WY DEQ approvals of the sewer and water plans, prior to their construction.

The DEQ issued Notice of Coverage, Permit No. 19-140 on May 13, 2019. A copy of the permit is available upon request.

6. The project must otherwise comply with the approved site plan and applicable building, fire, and electrical codes.

Noted.

7. A building permit must be obtained within three years or this authorization will expire.

Noted.

In addition, notes received from the Building Department have been addressed within the civil plans, and to our knowledge the department is satisfied and will issue a building permit upon approval from P&Z.

Thank you for your help during this long process and let us know if you have further concerns so we can get them addressed asap.

Sincerely,

A handwritten signature in blue ink that reads "David R. Shultz". The signature is written in a cursive, flowing style.

David R. Shultz, P.E.

Enclosure: Revised Plan Set

GENERAL NOTES

1. SPECIFIC RESOURCES, TECHNICAL REPORTS, DESIGN DOCUMENTS, ET, AL, RELATED TO THIS PROJECT INCLUDE (BUT MAY NOT BE LIMITED TO) THE FOLLOWING:
 -SURVEY: "RECORD OF SURVEY BOUNDARY LINE ADJUSTMENT" PREPARED BY ENGINEERING ASSOCIATES, P.O. BOX 1900, CODY, WY 82414, DATED 8-3-2018.
 -GEOTECHNICAL: "GEOTECHNICAL INVESTIGATION REPORT, PROPOSED HAMPTON INN, 8 SOUTH-FORK ROAD, CODY, WYOMING" PREPARED BY GEOSCIENCE, PLLP, 2728 GREGORY DRIVE NORTH, BILLINGS, MT 59102, DATED 8-17-2018.
 -WYDOT PERMITS: ACCESS, LANDSCAPING, AND FENCE REMOVAL AGREEMENTS.
 CONTACT WYDOT MAINTENANCE FOREMAN IN CODY: 307-587-2220.
 CONTRACTOR IS RESPONSIBLE FOR OBTAINING THESE DOCUMENTS AND FAMILIARIZING THEMSELVES WITH SAME FOR APPLICATION BOTH PRIOR TO AND DURING CONSTRUCTION.

2. ENSURE COMPLETE ADHERENCE TO AND COMPLETION OF ANY/ALL CERTIFICATIONS, INSPECTIONS, ETC. REQUIRED BY ALL GOVERNING JURISDICTIONAL AGENCIES DURING AND AFTER CONSTRUCTION FOR SIGN-OFF AND CERTIFICATE OF OCCUPANCY ISSUANCE, INCLUDING BUT NOT LIMITED TO PROCUREMENT OF SERVICES, SCHEDULING OF FIELD OBSERVATIONS AND COORDINATION WITH REPRESENTATIVES OF THE APPROPRIATE PARTIES.

3. DO NOT BURY DEBRIS ON THE SUBJECT SITE AND DISPOSE OF ALL UNSUITABLE EXCAVATED HAZARDOUS MATERIAL, AND DEBRIS (SOLID WASTE) IN ACCORDANCE WITH ALL CITY, COUNTY, STATE, AND FEDERAL LAWS AND APPLICABLE CODES.

4. ENSURE ALL SHORING REQUIRED DURING EXCAVATION IS PERFORMED IN ACCORDANCE WITH CURRENT OSHA STANDARDS AND ANY ADDITIONAL PROVISIONS AS FIELD CONDITIONS DICTATE.

5. DOCUMENT ALL EXISTING DAMAGE AND NOTIFY CONSTRUCTION MANAGER PRIOR TO START OF CONSTRUCTION. REPAIR DAMAGE DONE TO ANY EXISTING ITEM DURING CONSTRUCTION SUCH AS BUT NOT LIMITED TO: DRAINAGE, UTILITIES, PAVEMENT STRIPING, CURB, ETC.

6. DETERMINE METHODS MEANS FOR COMPLETION OF THE WORK PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. NOTIFY OWNER AND ENGINEER OF RECORD WHEN A CONFLICT IS IDENTIFIED.

7. ENSURE ALL CONTRACTORS CARRY STATUTORY WORKERS COMPENSATION INSURANCE, EMPLOYERS LIABILITY INSURANCE AND APPROPRIATE LIMITS OF COMMERCIAL GENERAL LIABILITY INSURANCE (CGL). ENSURE ALL CONTRACTORS HAVE THEIR CGL POLICIES ENDORSED TO NAME SAGE CIVIL ENGINEERING, AND ITS SUB-CONSULTANTS AS ADDITIONAL INSURED(S) AND PROVIDE CONTRACTUAL LIABILITY COVERAGE SUFFICIENT TO INSURE THE HOLD HARMLESS AND INDEMNITY OBLIGATIONS ASSUMED BY THE CONTRACTORS. ENSURE ALL CONTRACTORS FURNISH SAGE CIVIL ENGINEERING WITH CERTIFICATIONS OF INSURANCE AS EVIDENCE OF THE REQUIRED INSURANCE PRIOR TO COMMENCING WORK AND UPON RENEWAL OF EACH POLICY DURING THE ENTIRE PERIOD OF CONSTRUCTION. IN ADDITION, ENSURE ALL CONTRACTORS, TO THE FULLEST EXTENT PERMITTED BY LAW, INDEMNIFY AND HOLD HARMLESS SAGE CIVIL ENGINEERING AND ITS SUB-CONSULTANTS FROM AND AGAINST ANY DAMAGES, LIABILITIES OR COSTS, INCLUDING REASONABLE ATTORNEYS' FEES AND DEFENSE COSTS, ARISING OUT OF OR IN ANY WAY CONNECTED WITH THE PROJECT, INCLUDING ALL CLAIMS BY EMPLOYEES OF THE CONTRACTORS.

8. ALL CONTRACTORS SHALL BE LICENSED WITH THE CITY OF CODY BUILDING DEPARTMENT. ELECTRICAL CONTRACTORS SHALL ALSO BE LICENSED WITH THE STATE OF WYOMING.
 9. THE PROFESSIONAL ACTIVITIES OF SAGE CIVIL ENGINEERING, AND THE PRESENCE OF SAGE CIVIL ENGINEERING OR ITS EMPLOYEES AND SUB-CONSULTANTS AT A CONSTRUCTION/PROJECT SITE SHALL NOT RELIEVE THE GENERAL CONTRACTOR OF ITS OBLIGATIONS, DUTIES AND RESPONSIBILITIES INCLUDING, BUT NOT LIMITED TO, CONSTRUCTION MEANS, METHODS, SEQUENCE, TECHNIQUES OR PROCEDURES NECESSARY FOR PERFORMING, SUPERINTENDING AND COORDINATING THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND ANY HEALTH OR SAFETY PRECAUTIONS REQUIRED BY ANY REGULATORY AGENCIES. SAGE CIVIL ENGINEERING AND ITS PERSONNEL HAVE NO AUTHORITY TO EXERCISE ANY CONTROL OVER ANY CONSTRUCTION CONTRACTOR OR ITS EMPLOYEES IN CONNECTION WITH THEIR WORK OR ANY HEALTH OR SAFETY PROGRAMS OR PROCEDURES. THE GENERAL CONTRACTOR IS SOLELY RESPONSIBLE FOR JOB SITE SAFETY.

10. SUBMIT TO THE ENGINEER FOR REVIEW, APPROVAL, OR OTHER APPROPRIATE ACTION SHOP DRAWINGS, PRODUCT DATA, SAMPLES, AND OTHER DATA FOR ITEMS SPECIFIED ON THE CIVIL PLAN SHEETS. SUCH REVIEWS, ETC. ARE ONLY FOR THE LIMITED PURPOSE OF CHECKING FOR CONFORMANCE WITH THE DESIGN CONCEPT. REVIEWS WILL NOT BE PROVIDED FOR THE INFORMATION SHOWN IN THE CONSTRUCTION MEANS OR METHODS, COORDINATION OF THE WORK WITH OTHER TRADES, OR CONSTRUCTION SAFETY PRECAUTIONS. SAGE CIVIL ENGINEERING'S REVIEW SHALL BE CONDUCTED WITH REASONABLE PROMPTNESS. REVIEW OF A SPECIFIC ITEM WILL NOT INDICATE THAT SAGE CIVIL ENGINEERING HAS REVIEWED THE ENTIRE ASSEMBLY OF WHICH THE ITEM IS A COMPONENT. SAGE CIVIL ENGINEERING IS NOT RESPONSIBLE FOR ANY DEVIATIONS FROM THE CONSTRUCTION DOCUMENTS NOT BROUGHT TO THE ATTENTION OF SAGE CIVIL ENGINEERING IN WRITING BY THE CONTRACTOR. SAGE CIVIL ENGINEERING WILL NOT BE REQUIRED TO REVIEW PARTIAL SUBMISSIONS OR THOSE FOR WHICH SUBMISSIONS OF CORRELATED ITEMS HAVE NOT BEEN RECEIVED.

11. IN AN EFFORT TO RESOLVE ANY CONFLICTS THAT ARISE DURING THE DESIGN AND CONSTRUCTION OF THE PROJECT OR FOLLOWING THE COMPLETION OF THE PROJECT, SAGE CIVIL ENGINEERING AND/OR THE OWNER'S REPRESENTATIVE, AND THE CONTRACTOR MUST AGREE THAT ALL DISPUTES BETWEEN THEM ARISING OUT OF OR RELATING TO THIS PROJECT SHALL BE SUBMITTED TO NON-BINDING MEDIATION UNLESS THE PARTIES MUTUALLY AGREE OTHERWISE.

12. INCLUDE A MEDIATION PROVISION IN ALL AGREEMENTS WITH INDEPENDENT SUBCONTRACTORS AND CONSULTANTS RETAINED FOR THE PROJECT AND REQUIRE ALL INDEPENDENT CONTRACTORS AND CONSULTANTS ALSO TO INCLUDE A SIMILAR MEDIATION PROVISION IN ALL AGREEMENTS WITH THEIR SUBCONTRACTORS, SUB-CONSULTANTS, SUPPLIERS AND FABRICATORS, THEREBY PROVIDING FOR MEDIATION AS THE PRIMARY METHOD FOR DISPUTE RESOLUTION BETWEEN THE PARTIES TO ALL THOSE AGREEMENTS.

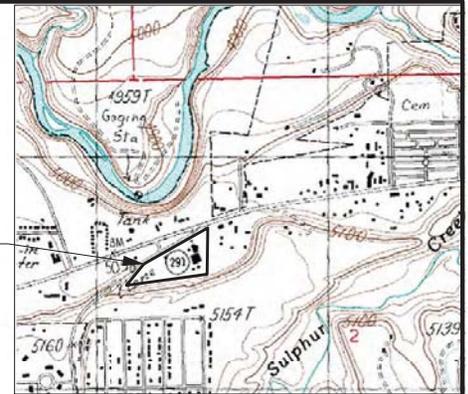
13. PAY ALL COSTS TO CORRECT ANY WORK COMPLETED. ALL FINES OR PENALTIES ASSESSED WITH RESPECT THERETO AND ALL COMPENSATORY OR PUNITIVE DAMAGES RESULTING FROM DEVIATING FROM THE PLANS AND SPECIFICATIONS, INCLUDING THE NOTES CONTAINED THEREON, IF PRIOR WRITTEN AUTHORIZATION FOR SUCH DEVIATIONS FROM THE OWNER AND ENGINEER IS NOT OBTAINED. INDEMNIFY AND HOLD THE OWNER AND ENGINEER HARMLESS FROM ALL SUCH COSTS TO CORRECT ANY SUCH WORK AND FROM ALL SUCH FINES AND PENALTIES, COMPENSATION AND PUNITIVE DAMAGES AND COSTS OF ANY NATURE RESULTING THEREFROM.

SITE DEVELOPMENT PLANS FOR HAMPTON INN & SUITES CODY, WYOMING

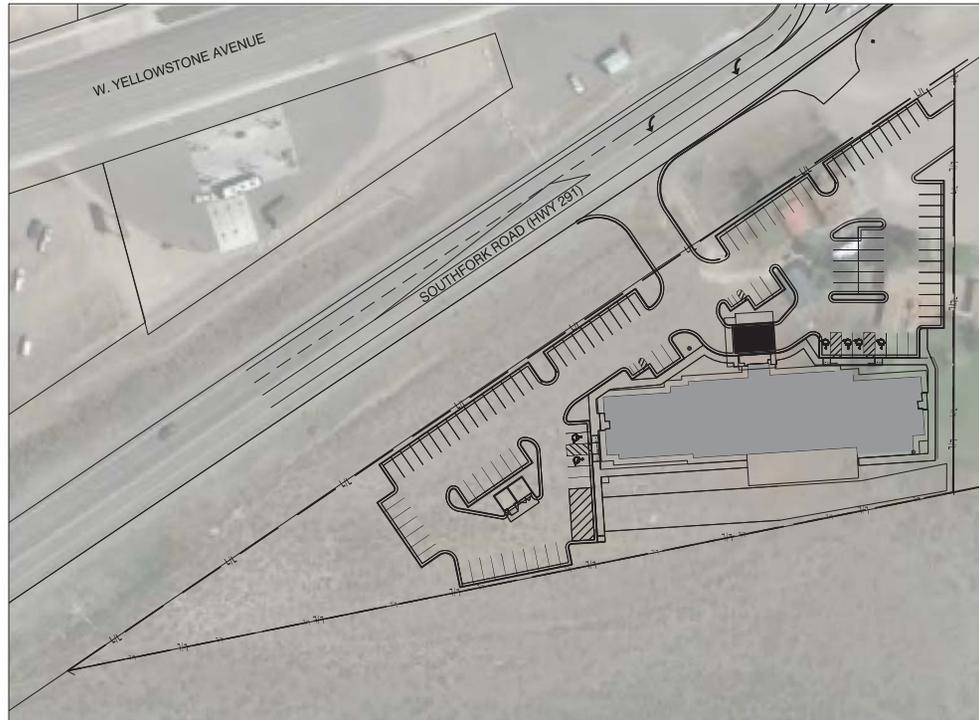
OWNER: JPK TR CODY LLC
P.O. BOX 15
ABERDEEN, SD 57402

AUGUST 30, 2019

PROJECT AREA



VICINITY MAP



CITY OF CODY ZONING DISTRICTS

- MOBILE HOME PARK (MHP)
- MEDIUM-LOW DENSITY RESIDENTIAL (R2)
- OPEN BUSINESS/LIGHT INDUSTRIAL (D3)
- INDUSTRIAL (E)

SHEET TITLE	SHEET NUMBER
DEMOLITION & EROSION/SEDIMENT CONTROL PLAN	1
SITE PLAN	2
GRADING PLAN	3
DRAINAGE PLAN	4
UTILITY PLAN	5
LANDSCAPE PLAN	6
SINGLE UNIT TRUCK TURNING RADIUS	7
CITY BUS TURNING RADIUS	8
FIRE TRUCK TURNING RADIUS	9
DETAILS	10-11

ALL FIRE MARSHAL AND PLANNING & ZONING REQUIREMENTS SHALL BE MET BEFORE ISSUANCE OF A CERTIFICATE OF OCCUPANCY



2824 BIG HORN AVE.
CODY, WY 82414
PHONE: (307) 527-0915
WWW.SAGECIVILENGINEERING.COM

SAGE CIVIL ENGINEERING IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION.

UTILITY DEMOLITION NOTES

1. POWER: NOTIFY THE CITY OF CODY WHEN POWER SERVICES ARE TO BE REMOVED. THE CITY ELECTRICAL DEPARTMENT WILL REMOVE EXISTING CABLE, POLES, AND TRANSFORMERS.
2. SANITARY SEWER: CUT AND CAP EXISTING SANITARY SEWER SERVICES.
3. TREATED WATER SERVICE: CLOSE CORPORATION STOP, CUT AND REMOVE SERVICE LINES.
4. TELEPHONE: DISCONNECT EXISTING TELEPHONE CABLES AT PEDESTAL. NOTIFY CENTURY LINK (307-587-4287) OF ACTIVITIES PRIOR TO DISCONNECTING.
5. GAS: EXPOSE SERVICE LINES AT THE MAIN. CONTACT ENERGY WEST (307-587-4281) TO DISCONNECT SERVICE.

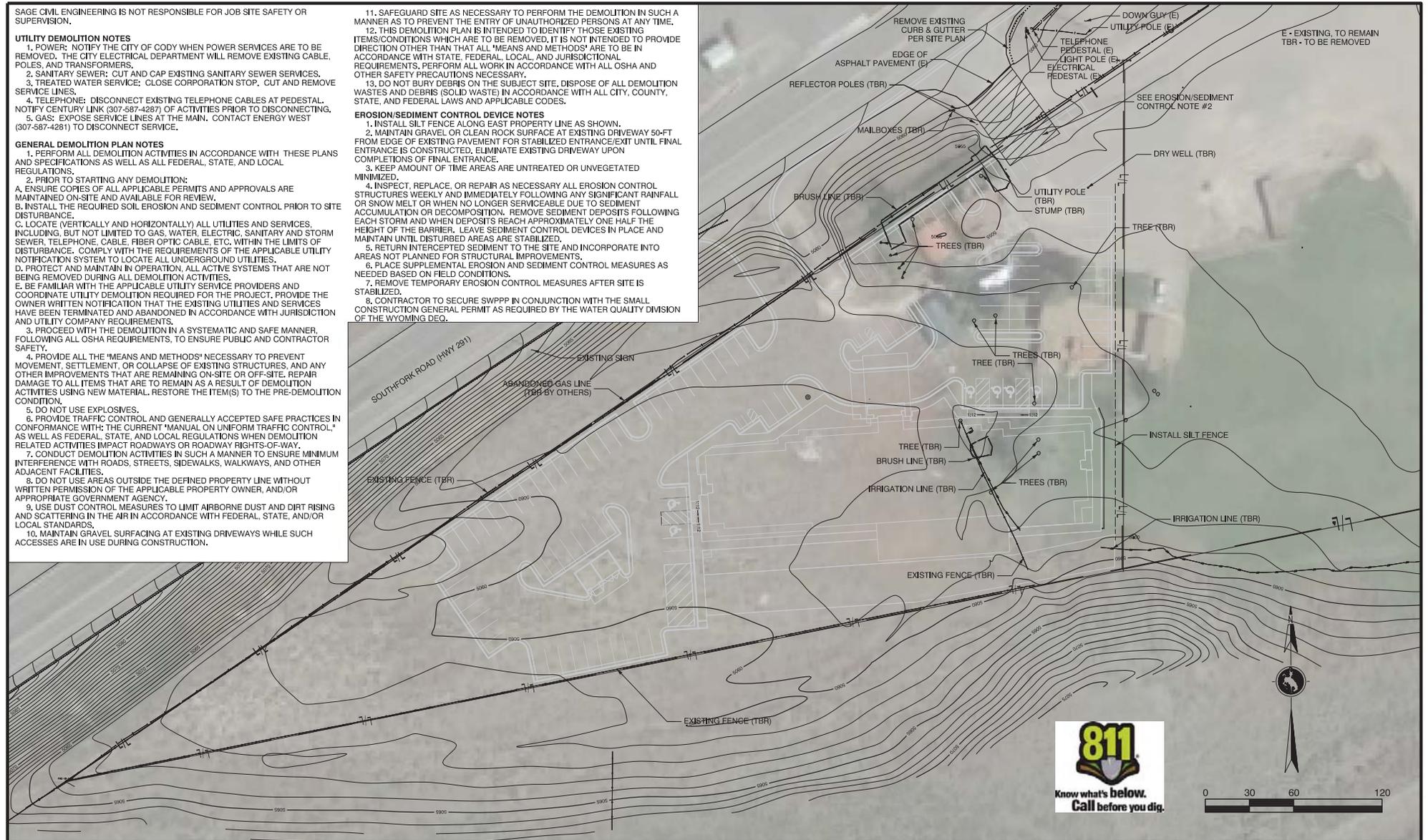
GENERAL DEMOLITION PLAN NOTES

1. PERFORM ALL DEMOLITION ACTIVITIES IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AS WELL AS ALL FEDERAL, STATE, AND LOCAL REGULATIONS.
2. PRIOR TO STARTING ANY DEMOLITION:
 - A. ENSURE COPIES OF ALL APPLICABLE PERMITS AND APPROVALS ARE MAINTAINED ON-SITE AND AVAILABLE FOR REVIEW.
 - B. INSTALL THE REQUIRED SOIL EROSION AND SEDIMENT CONTROL PRIOR TO SITE DISTURBANCE.
 - C. LOCATE (VERTICALLY AND HORIZONTALLY) ALL UTILITIES AND SERVICES, INCLUDING, BUT NOT LIMITED TO GAS, WATER, ELECTRIC, SANITARY AND STORM SEWER, TELEPHONE, CABLE, FIBER OPTIC CABLE, ETC. WITHIN THE LIMITS OF DISTURBANCE. COMPLY WITH THE REQUIREMENTS OF THE APPLICABLE UTILITY NOTIFICATION SYSTEM TO LOCATE ALL UNDERGROUND UTILITIES.
 - D. PROTECT AND MAINTAIN IN OPERATION, ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED DURING ALL DEMOLITION ACTIVITIES.
 - E. BE FAMILIAR WITH THE APPLICABLE UTILITY SERVICE PROVIDERS AND COORDINATE UTILITY DEMOLITION REQUIRED FOR THE PROJECT. PROVIDE THE OWNER WRITTEN NOTIFICATION THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY REQUIREMENTS.
3. PROCEED WITH THE DEMOLITION IN A SYSTEMATIC AND SAFE MANNER, FOLLOWING ALL OSHA REQUIREMENTS, TO ENSURE PUBLIC AND CONTRACTOR SAFETY.
4. PROVIDE ALL THE "MEANS AND METHODS" NECESSARY TO PREVENT MOVEMENT, SETTLEMENT, OR COLLAPSE OF EXISTING STRUCTURES, AND ANY OTHER IMPROVEMENTS THAT ARE REMAINING ON-SITE OR OFF-SITE. REPAIR DAMAGE TO ALL ITEMS THAT ARE TO REMAIN AS A RESULT OF DEMOLITION ACTIVITIES USING NEW MATERIAL. RESTORE THE ITEM(S) TO THE PRE-DEMOLITION CONDITION.
5. DO NOT USE EXPLOSIVES.
6. PROVIDE TRAFFIC CONTROL AND GENERALLY ACCEPTED SAFE PRACTICES IN CONFORMANCE WITH: THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL," AS WELL AS FEDERAL, STATE, AND LOCAL REGULATIONS WHEN DEMOLITION RELATED ACTIVITIES IMPACT ROADWAYS OR ROADWAY RIGHTS-OF-WAY.
7. CONDUCT DEMOLITION ACTIVITIES IN SUCH A MANNER TO ENSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, SIDEWALKS, WALKWAYS, AND OTHER ADJACENT FACILITIES.
8. DO NOT USE AREAS OUTSIDE THE DEFINED PROPERTY LINE WITHOUT WRITTEN PERMISSION OF THE APPLICABLE PROPERTY OWNER, AND/OR APPROPRIATE GOVERNMENT AGENCY.
9. USE DUST CONTROL MEASURES TO LIMIT AIRBORNE DUST AND DIRT RISING AND SCATTERING IN THE AIR IN ACCORDANCE WITH FEDERAL, STATE, AND/OR LOCAL STANDARDS.
10. MAINTAIN GRAVEL SURFACING AT EXISTING DRIVEWAYS WHILE SUCH ACCESSES ARE IN USE DURING CONSTRUCTION.

11. SAFEGUARD SITE AS NECESSARY TO PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE ENTRY OF UNAUTHORIZED PERSONS AT ANY TIME.
12. THIS DEMOLITION PLAN IS INTENDED TO IDENTIFY THOSE EXISTING ITEMS/CONDITIONS WHICH ARE TO BE REMOVED. IT IS NOT INTENDED TO PROVIDE DIRECTION OTHER THAN THAT ALL "MEANS AND METHODS" ARE TO BE IN ACCORDANCE WITH STATE, FEDERAL, LOCAL, AND JURISDICTIONAL REQUIREMENTS. PERFORM ALL WORK IN ACCORDANCE WITH ALL OSHA AND OTHER SAFETY PRECAUTIONS NECESSARY.
13. DO NOT BURY DEBRIS ON THE SUBJECT SITE. DISPOSE OF ALL DEMOLITION WASTES AND DEBRIS (SOLID WASTE) IN ACCORDANCE WITH ALL CITY, COUNTY, STATE, AND FEDERAL LAWS AND APPLICABLE CODES.

EROSION/SEDIMENT CONTROL DEVICE NOTES

1. INSTALL SILT FENCE ALONG EAST PROPERTY LINE AS SHOWN.
2. MAINTAIN GRAVEL OR CLEAN ROCK SURFACE AT EXISTING DRIVEWAY 50-FT FROM EDGE OF EXISTING PAVEMENT FOR STABILIZED ENTRANCE/EXIT UNTIL FINAL ENTRANCE IS CONSTRUCTED. ELIMINATE EXISTING DRIVEWAY UPON COMPLETION OF FINAL ENTRANCE.
3. KEEP AMOUNT OF TIME AREAS ARE UNTREATED OR UNVEGETATED MINIMIZED.
4. INSPECT, REPLACE, OR REPAIR AS NECESSARY ALL EROSION CONTROL STRUCTURES WEEKLY AND IMMEDIATELY FOLLOWING ANY SIGNIFICANT RAINFALL OR SNOW MELT OR WHEN NO LONGER SERVICEABLE DUE TO SEDIMENT ACCUMULATION OR DECOMPOSITION. REMOVE SEDIMENT DEPOSITS FOLLOWING EACH STORM AND WHEN DEPOSITS REACH APPROXIMATELY ONE HALF THE HEIGHT OF THE BARRIER. LEAVE SEDIMENT CONTROL DEVICES IN PLACE AND MAINTAIN UNTIL DISTURBED AREAS ARE STABILIZED.
5. RETURN INTERCEPTED SEDIMENT TO THE SITE AND INCORPORATE INTO AREAS NOT PLANNED FOR STRUCTURAL IMPROVEMENTS.
6. PLACE SUPPLEMENTAL EROSION AND SEDIMENT CONTROL MEASURES AS NEEDED BASED ON FIELD CONDITIONS.
7. REMOVE TEMPORARY EROSION CONTROL MEASURES AFTER SITE IS STABILIZED.
8. CONTRACTOR TO SECURE SWPPP IN CONJUNCTION WITH THE SMALL CONSTRUCTION GENERAL PERMIT AS REQUIRED BY THE WATER QUALITY DIVISION OF THE WYOMING DEQ.



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8/30/2019	REVISED P&Z SUBMITTAL	CHECKED BY:	TSW
		APPROVED BY:	DIS

DEMOLITION & EROSION/SEDIMENT CONTROL PLAN

**HAMPTON INN & SUITES
JPK TR CODY, LLC**



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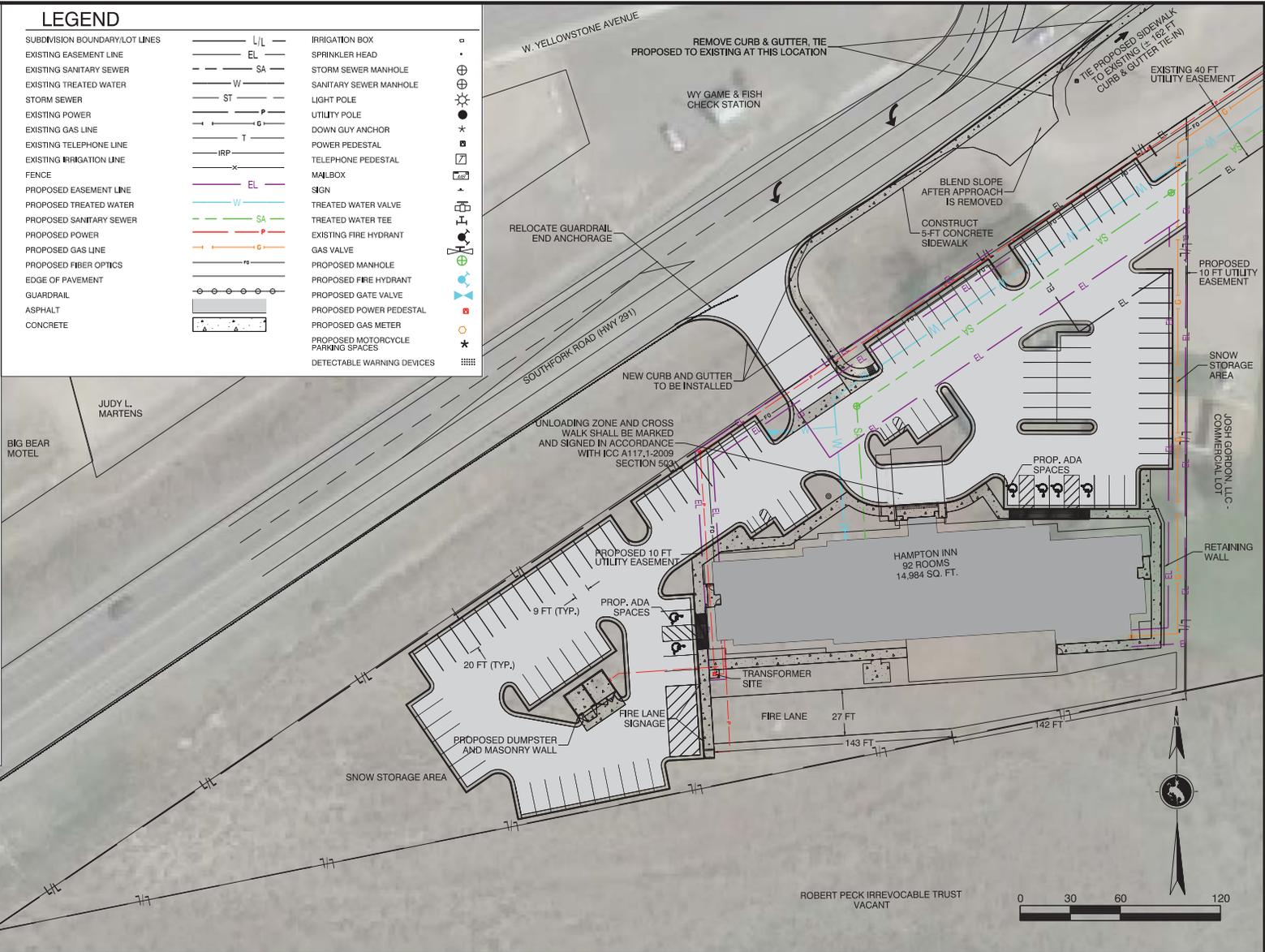
SAGE CIVIL ENGINEERING IS NOT RESPONSIBLE FOR CONSTRUCTION METHODS/MEANS FOR COMPLETION OF THE WORK DEPICTED ON THESE PLANS OR ANY CONFLICTS OR SCOPE REVISIONS WHICH RESULT FROM SAME. SAGE CIVIL ENGINEERING IS NOT RESPONSIBLE FOR JOB SITE SAFETY NOR HAVE THEY BEEN RETAINED FOR SUCH PURPOSES.

SITE PLAN NOTES

- THESE PLANS REFERENCE A SURVEY PREPARED BY: ENGINEERING ASSOCIATES P.O. BOX 1900 CODY, WY 82414 DATED: 8-3-2018
- COMPLETE ALL WORK WITHIN THE HIGHWAY RIGHT-OF-WAY AND ACCESS IN ACCORDANCE WITH THE APPROVED WYDOT ACCESS PERMIT. BEFORE BEGINNING WORK, CONTACT WYDOT CREW FOREMAN, CODY, 307-587-2220. PROVIDE A TRAFFIC CONTROL PLAN IN ACCORDANCE WITH WYDOT'S TRAFFIC CONTROL FOR ROADWAY WORK OPERATIONS MANUAL. AFTER COMPLETION, CONTACT ABOVE PERSON FOR INSPECTION OF THE WORK.
- CONSTRUCT CURB & GUTTER IN HIGHWAY RIGHT-OF-WAY IN CONFORMANCE WITH WYDOT STANDARD PLAN 608-01A.
- ALL ELEVATIONS SHOWN ARE IN REFERENCE TO SAGE CIVIL ENGINEERING'S BENCHMARK(S) AND MUST BE VERIFIED BY THE GENERAL CONTRACTOR PRIOR TO GROUND BREAK.
- CONSTRUCT ALL ADA PARKING FACILITIES TO MEET CURRENT ADA REQUIREMENTS. ENSURE DETECTABLE WARNING DEVICES CONFORM TO WYDOT STANDARD PLAN 608-01A.
- PRIOR TO STARTING CONSTRUCTION, ENSURE ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED.
- PERFORM ALL WORK IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND THE REQUIREMENTS AND STANDARDS OF THE LOCAL GOVERNING AUTHORITY.
- FOR ADA PARKING AND STOP SIGN PANELS, USE 0.125-INCH THICK SHEET ALUMINUM MADE USING 5052-H38 OR 6061-T6 ALLOY IN ACCORDANCE WITH ASTM B 209. FOR REFLECTIVE SHEETING, USE AN ENCAPSULATED GLASS BEAD, ENCAPSULATED PRISMATIC, OR NONMETALLIC, MICROPRISMATIC REFLECTIVE MATERIAL PRECOATED WITH ADHESIVE BACKING PROTECTED BY A TREATED PLASTIC LINER AND IN ACCORDANCE WITH ASHTO M 288 (ASTM D 4956).
- USE COMMERCIAL MANUFACTURED HIGHWAY STRIPING PAINT, APPLY AT A WET-PAINT THICKNESS OF 16 MIL.
- THE GEOTECHNICAL REPORT AND RECOMMENDATIONS SET FORTH THEREIN IS A PART OF THE REQUIRED CONSTRUCTION DOCUMENTS AND IN CASE OF CONFLICT SHALL TAKE PRECEDENCE UNLESS SPECIFICALLY NOTED OTHERWISE ON THE PLANS. NOTIFY THE ENGINEER IN WRITING OF ANY SUCH DISCREPANCY BETWEEN GEOTECHNICAL REPORT AND PLANS, ETC.
- THE PROPERTY SURVEY SHALL BE CONSIDERED A PART OF THESE PLANS.
- THESE PLANS ARE BASED ON INFORMATION PROVIDED TO SAGE CIVIL ENGINEERING AT THE TIME OF PLAN PREPARATION. FIELD VERIFY EXISTING CONDITIONS AND NOTIFY SAGE CIVIL ENGINEERING IF ACTUAL SITE CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLAN, OR IF THE PROPOSED WORK WOULD BE INHIBITED BY ANY OTHER SITE FEATURES.
- VERIFY ALL DIMENSIONS SHOWN ON THE PLANS PRIOR TO CONSTRUCTION. NOTIFY ENGINEER IN WRITING IF ANY DISCREPANCIES EXIST PRIOR TO PROCEEDING WITH CONSTRUCTION. NO EXTRA COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
- REFER TO THE ARCHITECTURAL/BUILDING PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF ENTRY/EXIT POINTS, ELEVATIONS, PRECISE BUILDING DIMENSIONS, EXACT BUILDING UTILITY LOCATIONS.
- USE CONCRETE MEETING THE REQUIREMENTS OF WYOMING PUBLIC WORKS STANDARD SPECIFICATIONS CLASS 4000 UNLESS OTHERWISE NOTED ON THE PLANS, DETAILS AND/OR GEOTECHNICAL REPORT.
- USE CRUSHED BASE MEETING THE REQUIREMENTS OF WYOMING PUBLIC WORKS STANDARD SPECIFICATIONS GRADING W, OR AS APPROVED BY THE OWNER.

LEGEND

SUBDIVISION BOUNDARY/LOT LINES	L/L	IRRIGATION BOX	○
EXISTING EASEMENT LINE	EL	SPRINKLER HEAD	⊙
EXISTING SANITARY SEWER	SA	STORM SEWER MANHOLE	⊕
EXISTING TREATED WATER	W	SANITARY SEWER MANHOLE	⊕
STORM SEWER	ST	LIGHT POLE	⊙
EXISTING POWER	P	UTILITY POLE	⊙
EXISTING GAS LINE	G	DOWN GUY ANCHOR	⊙
EXISTING TELEPHONE LINE	T	POWER PEDESTAL	⊙
EXISTING IRRIGATION LINE	IRP	TELEPHONE PEDESTAL	⊙
FENCE	X	MAILBOX	⊙
PROPOSED EASEMENT LINE	EL	SIGN	⊙
PROPOSED TREATED WATER	W	TREATED WATER VALVE	⊙
PROPOSED SANITARY SEWER	SA	TREATED WATER TEE	⊙
PROPOSED POWER	P	EXISTING FIRE HYDRANT	⊙
PROPOSED GAS LINE	G	GAS VALVE	⊙
PROPOSED FIBER OPTICS	F	PROPOSED MANHOLE	⊙
EDGE OF PAVEMENT	—	PROPOSED FIRE HYDRANT	⊙
GUARDRAIL	—	PROPOSED GATE VALVE	⊙
ASPHALT	—	PROPOSED POWER PEDESTAL	⊙
CONCRETE	—	PROPOSED GAS METER	⊙
		PROPOSED MOTORCYCLE PARKING SPACES	⊙
		DETECTABLE WARNING DEVICES	⊙



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		APPROVED BY: DRS

SITE PLAN

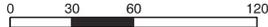
**HAMPTON INN & SUITES
JPK TR CODY, LLC**

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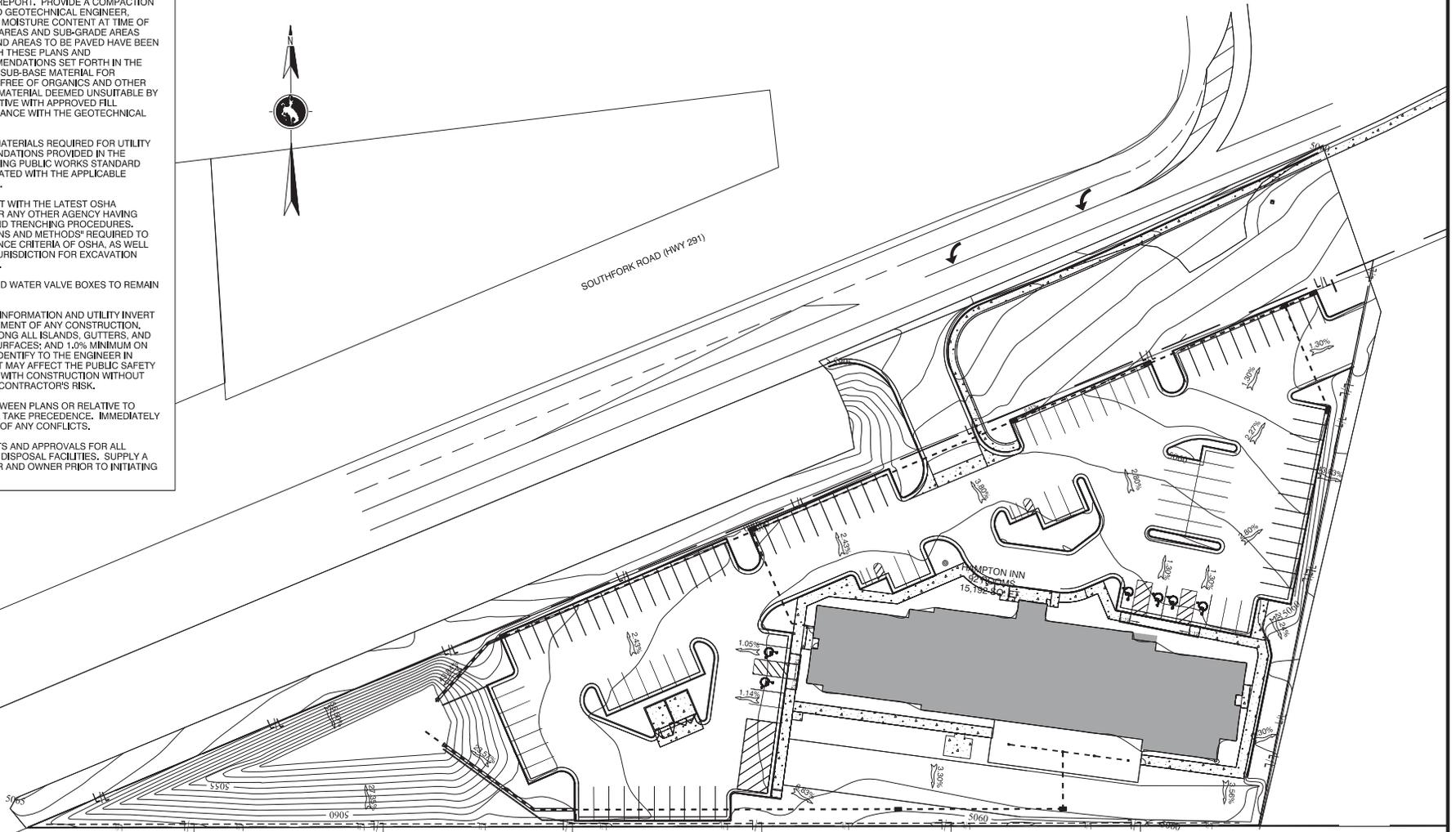


GRADING NOTES

1. PERFORM SITE GRADING IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND THE RECOMMENDATIONS SET FORTH IN THE GEOTECHNICAL REPORT REFERENCED IN THIS PLAN SET. REMOVE AND REPLACE WITH SUITABLE MATERIALS AS SPECIFIED IN THE GEOTECHNICAL REPORT. COMPACT EXCAVATED OR FILLED AREAS AS OUTLINED IN THE GEOTECHNICAL REPORT. PROVIDE A COMPACTION REPORT PREPARED BY A QUALIFIED GEOTECHNICAL ENGINEER, LICENSED IN WYOMING, VERIFYING MOISTURE CONTENT AT TIME OF PLACEMENT AND THAT ALL FILLED AREAS AND SUB-GRADE AREAS WITHIN THE BUILDING PAD AREA AND AREAS TO BE PAVED HAVE BEEN COMPACTED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND THE RECOMMENDATIONS SET FORTH IN THE GEOTECHNICAL REPORT. ENSURE SUB-BASE MATERIAL FOR SIDEWALKS, CURB, OR ASPHALT IS FREE OF ORGANICS AND OTHER UNSUITABLE MATERIALS. REMOVE MATERIAL DEEMED UNSUITABLE BY OWNER OR OWNER'S REPRESENTATIVE WITH APPROVED FILL MATERIAL COMPACTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT.
2. PROVIDE AND PLACE BACKFILL MATERIALS REQUIRED FOR UTILITY INSTALLATION PER THE RECOMMENDATIONS PROVIDED IN THE GEOTECHNICAL REPORT OR WYOMING PUBLIC WORKS STANDARD SPECIFICATIONS AND AS COORDINATED WITH THE APPLICABLE UTILITY COMPANY SPECIFICATIONS.
3. COMPLY TO THE FULLEST EXTENT WITH THE LATEST OSHA STANDARDS AND REGULATIONS, OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. DETERMINE AND UTILIZE THE "MEANS AND METHODS" REQUIRED TO MEET THE INTENT AND PERFORMANCE CRITERIA OF OSHA, AS WELL AS ANY OTHER ENTITY THAT HAS JURISDICTION FOR EXCAVATION AND/OR TRENCHING PROCEDURES.
4. ADJUST EXISTING MANHOLES AND WATER VALVE BOXES TO REMAIN TO MATCH FINAL GRADES.
5. VERIFY EXISTING TOPOGRAPHIC INFORMATION AND UTILITY INVERT ELEVATIONS PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. ENSURE 0.50% MINIMUM SLOPE ALONG ALL ISLANDS, GUTTERS, AND CURBS; 1.0% ON ALL CONCRETE SURFACES; AND 1.0% MINIMUM ON ASPHALT, TO PREVENT PONDING. IDENTIFY TO THE ENGINEER IN WRITING ANY DISCREPANCIES THAT MAY AFFECT THE PUBLIC SAFETY OR PROJECT COST. PROCEEDING WITH CONSTRUCTION WITHOUT NOTIFICATION IS DONE SO AT THE CONTRACTORS RISK.
6. IN CASE OF DISCREPANCIES BETWEEN PLANS OR RELATIVE TO OTHER PLANS, THE SITE PLAN WILL TAKE PRECEDENCE. IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY CONFLICTS.
7. SECURE ALL NECESSARY PERMITS AND APPROVALS FOR ALL OFF-SITE MATERIAL SOURCES AND DISPOSAL FACILITIES. SUPPLY A COPY OF APPROVALS TO ENGINEER AND OWNER PRIOR TO INITIATING WORK.



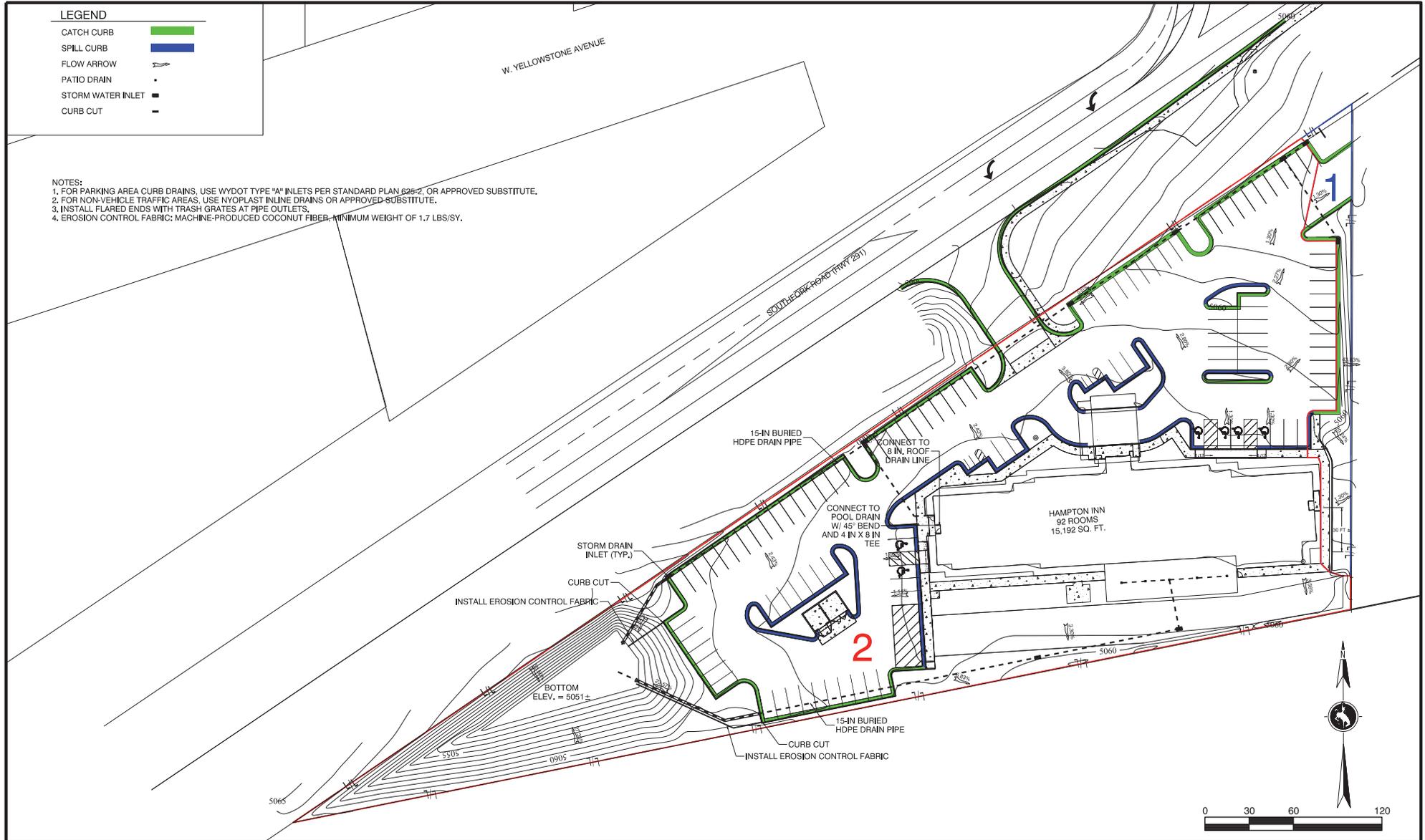
SOUTH FORK ROAD (HWY 291)



LEGEND

- CATCH CURB
- SPILL CURB
- FLOW ARROW
- PATIO DRAIN
- STORM WATER INLET
- CURB CUT

- NOTES:**
1. FOR PARKING AREA CURB DRAINS, USE WYDOT TYPE "A" INLETS PER STANDARD PLAN 628-2, OR APPROVED SUBSTITUTE.
 2. FOR NON-VEHICLE TRAFFIC AREAS, USE NYOPLAST INLINE DRAINS OR APPROVED SUBSTITUTE.
 3. INSTALL FLARED ENDS WITH TRASH GRATES AT PIPE OUTLETS.
 4. EROSION CONTROL FABRIC: MACHINE-PRODUCED COCONUT FIBER, MINIMUM WEIGHT OF 1.7 LBS/SY.



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DRAINAGE PLAN

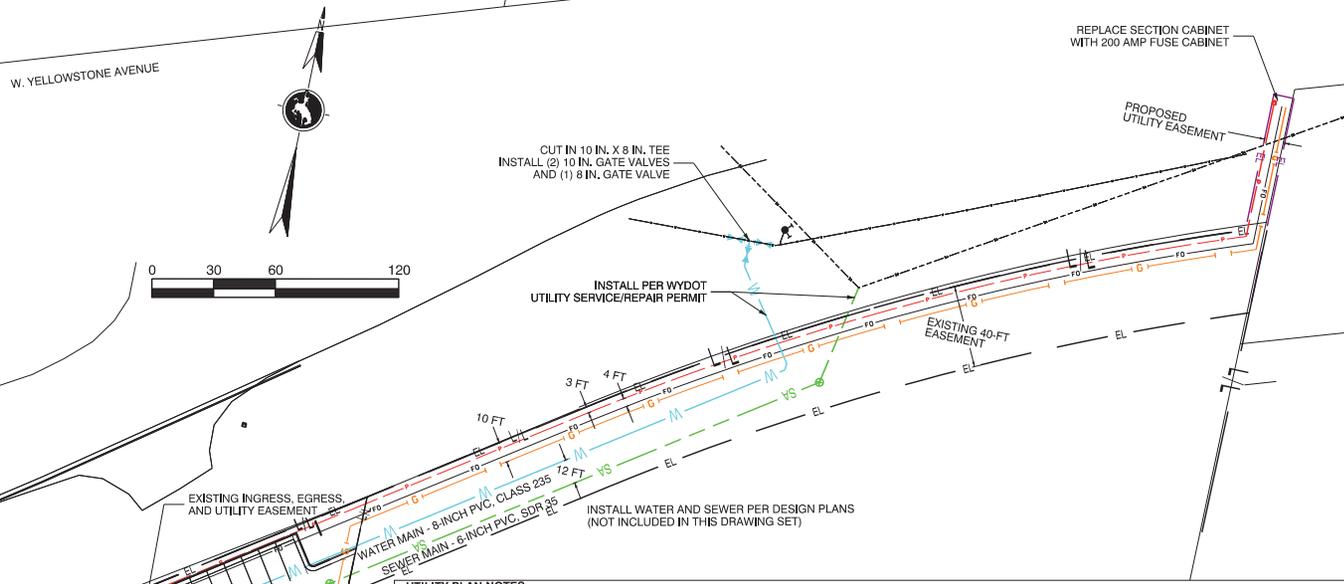
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LEGEND

SUBDIVISION BOUNDARY/LOT LINES	— L/L —	IRRIGATION BOX	○
EXISTING EASEMENT LINE	— EL —	SPRINKLER HEAD	⊕
EXISTING SANITARY SEWER	— SA —	STORM SEWER MANHOLE	⊕
EXISTING TREATED WATER	— W —	SANITARY SEWER MANHOLE	⊕
STORM SEWER	— ST —	LIGHT POLE	⊙
EXISTING POWER	— P —	UTILITY POLE	⊙
EXISTING GAS LINE	— G —	DOWN GUY ANCHOR	⊙
EXISTING TELEPHONE LINE	— T —	POWER PEDESTAL	⊙
EXISTING IRRIGATION LINE	— IRP —	TELEPHONE PEDESTAL	⊙
FENCE	— X —	MAILBOX	⊙
PROPOSED EASEMENT LINE	— EL —	SIGN	⊙
PROPOSED TREATED WATER	— W —	TREATED WATER VALVE	⊙
PROPOSED SANITARY SEWER	— SA —	TREATED WATER TEE	⊙
PROPOSED POWER	— P —	EXISTING FIRE HYDRANT	⊙
PROPOSED GAS LINE	— G —	GAS VALVE	⊙
PROPOSED FIBER OPTICS	— FO —	PROPOSED MANHOLE	⊙
EDGE OF PAVEMENT	— E —	PROPOSED FIRE HYDRANT	⊙
GUARDRAIL	— G —	PROPOSED GATE VALVE	⊙
ASPHALT	— A —	PROPOSED POWER PEDESTAL	⊙
CONCRETE	— C —	PROPOSED GAS METER	⊙
		PROPOSED MOTORCYCLE PARKING SPACES	⊙
		DETECTABLE WARNING DEVICES	⊙



UTILITY PLAN NOTES

1. LOCATIONS OF ALL EXISTING AND PROPOSED SERVICES ARE APPROXIMATE. CONFIRM WITH LOCAL UTILITY COMPANIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION OR EXCAVATION. CONFIRM ALL UTILITY SERVICE CONNECTION POINTS IN FIELD PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. REPORT DISCREPANCIES IMMEDIATELY IN WRITING TO THE ENGINEER. FIELD VERIFY ALL PROPOSED UTILITY CROSSINGS WITH EXISTING UNDERGROUND UTILITIES PRIOR TO COMMENCEMENT OF CONSTRUCTION.
2. INSTALL UTILITIES IN ACCORDANCE WITH CITY OF CODY STANDARD DETAILS AND/OR WYOMING PUBLIC WORKS STANDARD SPECIFICATIONS (WPWSS) AND DETAILS. THESE NOTES TAKE PRECEDENCE OVER STANDARDS. IF CONFLICTING, CURRENT CITY OF CODY CONSTRUCTION DETAILS ARE AVAILABLE ON THE CITY'S WEBSITE: www.cityofcody.wy.gov.
3. REFERENCE THE CITY OF CODY ELECTRICAL DIVISION 2018 ELECTRICAL DISTRIBUTION STANDARDS MANUAL.
4. VERTICALLY AND HORIZONTALLY LOCATE ALL UTILITIES AND SERVICES INCLUDING BUT NOT LIMITED TO GAS, WATER, ELECTRIC, SANITARY AND STORM SEWER, TELEPHONE, CABLE, FIBER OPTIC CABLE, ETC. WITHIN THE PROJECT LIMITS. USE AND COMPLY WITH THE REQUIREMENTS OF THE APPLICABLE UTILITY NOTIFICATION SYSTEM TO LOCATE ALL THE UNDERGROUND UTILITIES. REPAIR AT NO COST TO THE OWNER DAMAGES TO ANY EXISTING UTILITIES CAUSED DURING CONSTRUCTION.
5. REVIEW ALL DRAWINGS AND SPECIFICATIONS ASSOCIATED WITH THE PROJECT SCOPE OF WORK PRIOR TO THE INITIATION OF CONSTRUCTION. NOTIFY SAGE CIVIL ENGINEERING IN WRITING PRIOR TO THE START OF CONSTRUCTION OF CONFLICTS WITH THE DOCUMENTS RELATIVE TO THE SPECIFICATIONS OR THE RELATIVE CODES. FAILURE BY THE CONTRACTOR TO NOTIFY THE ENGINEER SHALL CONSTITUTE ACCEPTANCE OF FULL RESPONSIBILITY TO COMPLETE THE SCOPE OF WORK AS DEFINED BY THE DRAWINGS AND IN FULL COMPLIANCE WITH LOCAL REGULATIONS AND CODES.
6. DEFINE AND LOCATE VERTICALLY AND HORIZONTALLY ALL ACTIVE UTILITY AND/OR SERVICE SYSTEMS THAT ARE TO BE REMOVED. PROTECT AND MAINTAIN ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED/RELOCATED DURING SITE ACTIVITY.
7. ENSURE FAMILIARITY WITH APPLICABLE UTILITY SERVICE PROVIDER REQUIREMENTS. COORDINATE WITH UTILITY SERVICE PROVIDERS REGARDING UTILITY DEMOLITION AS IDENTIFIED OR REQUIRED FOR PROJECT. PROVIDE THE OWNER WRITTEN NOTIFICATION THAT EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION UTILITY REQUIREMENTS AND COORDINATE THE UTILITY TIE-INS/CONNECTIONS PRIOR TO CONNECTING TO THE EXISTING UTILITY/SERVICE. NOTIFY ENGINEER OF CONFLICTS WITH THESE PLANS PRIOR TO CONSTRUCTION.
8. COORDINATE SITE PLAN DOCUMENTS AND ARCHITECTURAL DESIGN FOR EXACT BUILDING UTILITY CONNECTION LOCATIONS. COORDINATE INSTALLATION OF UTILITIES/SERVICES WITH THE INDIVIDUAL COMPANIES TO AVOID CONFLICTS AND ENSURE PROPER DEPTHS ARE ACHIEVED. MEET JURISDICTION UTILITY REQUIREMENTS AND COORDINATE THE UTILITY TIE-INS/CONNECTIONS PRIOR TO CONNECTING TO THE EXISTING UTILITY/SERVICE. NOTIFY ENGINEER OF CONFLICTS WITH THESE PLANS PRIOR TO CONSTRUCTION.
9. INSTALL WATER AND SEWER MAINS IN ACCORDANCE WITH APPROVED PLANS. MAINTAIN A MINIMUM OF 10-FT HORIZONTAL SEPARATION BETWEEN TREATED WATER AND SANITARY SEWER MAINS.
10. INSTALL ALL NEW UTILITIES/SERVICES, INCLUDING ELECTRIC, TELEPHONE, CABLE TV, ETC. UNDERGROUND IN ACCORDANCE WITH THE UTILITY/SERVICE PROVIDER INSTALLATION SPECIFICATIONS AND STANDARDS.
11. USE POLYVINYL CHLORIDE (PVC) PRESSURE PIPE, CLASS 235.
12. USE VALVES APPROVED BY THE CITY OF CODY.
13. ENSURE CAST IRON FITTINGS ARE CLASS 350 AND CONFORM TO AWWA C 110 OR AWWA C153 FOR COMPACT FITTINGS.
14. PLACE COMMERCIALY AVAILABLE POLYETHYLENE TAPE MANUFACTURED FOR PROTECTION, EARLY IDENTIFICATION, AND ACCURATE LOCATION OF UNDERGROUND UTILITIES. ENSURE PROPER COLOR PER A.P.W.A. COLOR CODE. CITY OF CODY WILL PROVIDE TAPE FOR ELECTRICAL TRENCH. PLACE ±18 INCHES ABOVE CONDUIT.
15. USE 10 AWG INSULATED, NYLON JACKETED TRACE WIRE COPPER CONDUCTOR.
16. USE 18-8 TYPE 304 STAINLESS STEEL TAPPING SLEEVE. USE FLANGES MEETING AWWA C207 CLASS D ANSI, 150 LB DRILLING. USE SLEEVES RATED TO 2500 PSI HYDROSTATIC WORKING PRESSURE AND TO WITHSTAND TEST PRESSURES IN ACCORDANCE WITH WPWSS SECTION 02670. HYDROSTATIC TESTING. USE 18-8 TYPE STAINLESS STEEL COATED BOLTS AND NUTS.
17. WHEN INSTALLING SEWER TIE-IN AT EXISTING MANHOLE, MATCH ALL PIPE ELEVATIONS WITH REBUILT TROUGHS IN THE BOTTOM OF THE MANHOLE AND SEAL PENETRATIONS.
18. REPLACE EXISTING SECTIONALIZING CABINET WHERE THE NEW LINE ORIGINATES WITH A 200-AMP FUSE CABINET TO PROVIDE FUSE PROTECTION FOR THE NEW LINE.
19. ADD SECTIONALIZING CABINET AT THE 880-FT POINT OF THE NEW LINE DUE TO KEEP CONDUIT SWEEPS PER CABLE RUN TO THREE, MAXIMUM.
20. A 500 KVA TRANSFORMER (208Y/120 VOLT SECONDARY) WILL BE DUE.
21. CONTRACTOR TO INSTALL 4-IN SCHEDULE 40 PVC CONDUIT WITH 36-IN RADIUS SWEEP TO MEET CITY SPECIFICATIONS. CONDUIT DEPTH SHALL BE 48-IN AND CONDUIT SHALL BE BEDDED (SEE DETAIL SHEET).
22. CONTRACTOR SHALL INSTALL THE FOLLOWING CITY-PROVIDED MATERIALS: 1) GROUND SLEEVE FOR THE SECTIONALIZING CABINET, 2) FIBERGLASS TRANSFORMER BOX PAD, AND 3) GROUND RODS FOR EACH LOCATION PER CITY SPECIFICATIONS.
23. UTILITY CONTACTS INCLUDE BUT NOT LIMITED TO:

CITY OF CODY	BLACK HILLS ENERGY	TCT WEST
307-527-7511	307-687-4281	307-586-3800
SPECTRUM	CENTURY LINK	
866-874-2389	877-837-5738	

2824 BIG HORN AVE.
CODY, WY 82414
PHONE: (307) 527-9815
WWW.SAGECIVILENGINEERING.COM



REVISION		DATE	DESCRIPTION
1	REVISED P&Z SUBMITTAL	08/20/2019	

JOB NO.	2017-17
DRAWN BY:	TSW
CHECKED BY:	TSW
APPROVED BY:	DIS

UTILITY PLAN

HAMPTON INN & SUITES JPK TR CODY, LLC

LEGEND

- EDGING
- IRRIGATION CONDUIT
- 3'-6" LANDSCAPE BOULDERS
- IRRIGATED TURF LAWN
- DRYLAND GRASS
- ROCK MULCH
- STORMWATER RETENTION AREA - COBBLE BOTTOM
- RIVER ROCK ON SIDES OF DRAIN SWALE
- WASHED ROCK

PLANT SCHEDULE:		
KEY	COMMON NAME	BOTANICAL NAME
TREE		
	FAT ALBERT SPRUCE	PICEA PUNGENS 'FAT ALBERT'
	AMUR MAPLE	ACER GINNALA
	BOULEVARD LINDEN	LILIA AMERICANA 'BOULEVARD'
	SKYLINE HONEYLOCUST	GLEDISIA TRIACANTHOS VAR. INERMIS 'SKYCOLE'
	QUAKING ASPEN	POPULUS TREMULOIDES
	DOUBLE FLOWERING PLUM	PRUNUS TRILOBA
SHRUBS		
	SKY HIGH JUNIPER	JUNIPERUS SCOOPULORUM 'BALLIGH'
	RABBIT BRUSH	CHRYSOTHAMNUS NAUSEOSUS
	DWARF RABBIT BRUSH	CHRYSOTHAMNUS NAUSEOSUS VAR. NAUSEOSUS
	BUFFALO JUNIPER	JUNIPERUS SABINA - BUFFALO
	FRAGRANT SUMAC	RHUS AROMATICA
	LITTLE MOSES BURNING BUSH	EUONYMUS ALATUS 'ODOM' LITTLE MOSES
	DIABLO NINEBARK	PHYSOCARPUS OPULIFOLIUS 'MONLO'
PERENNIALS AND ORNAMENTAL GRASSES		
	KARL FOERSTER FEATHER REED GRASS	CALAMAGROSIS ACUTIFLORA 'KARL FOERSTER'
	OVERDAM FEATHER REED GRASS	CALAMAGROSIS ACUTIFLORA 'OVERDAM'
	BLACK EYED SUSAN	RUDBECKIA FULGIDA 'GOLDSTURM'
	STELLA D'ORO DAYLILY	HEMEROCALLIS STELLA D'ORO
	BLUE OAT GRASS	HELICTOTRICHON SEMPERVIRENS
	MAY NIGHT SALVIA	SALVIA NEMROSA 'MAINACHT'

NOTES:
 1. FILL MATERIAL MAY NEED REPLACED WITH SOIL FOR LANDSCAPED AREAS DUE TO THE AMOUNT OF COBBLE PRESENT.
 2. LANDSCAPING SHALL PROVIDE CLEARANCES NOTED ON PAGE 30 OF THE ELECTRICAL DISTRIBUTION STANDARDS MANUAL.



DATE	REVISION	JOB NO.
08/30/2019	REVISED P&Z SUBMITTAL	2015-17
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		APPROVED BY: DMS

LANDSCAPE PLAN

HAMPTON INN & SUITES
 JPK TR CODY, LLC

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 CODY, WY 82414
 PHONE: (307) 527-8915
 WWW.SAGEGEMENGINEERING.COM

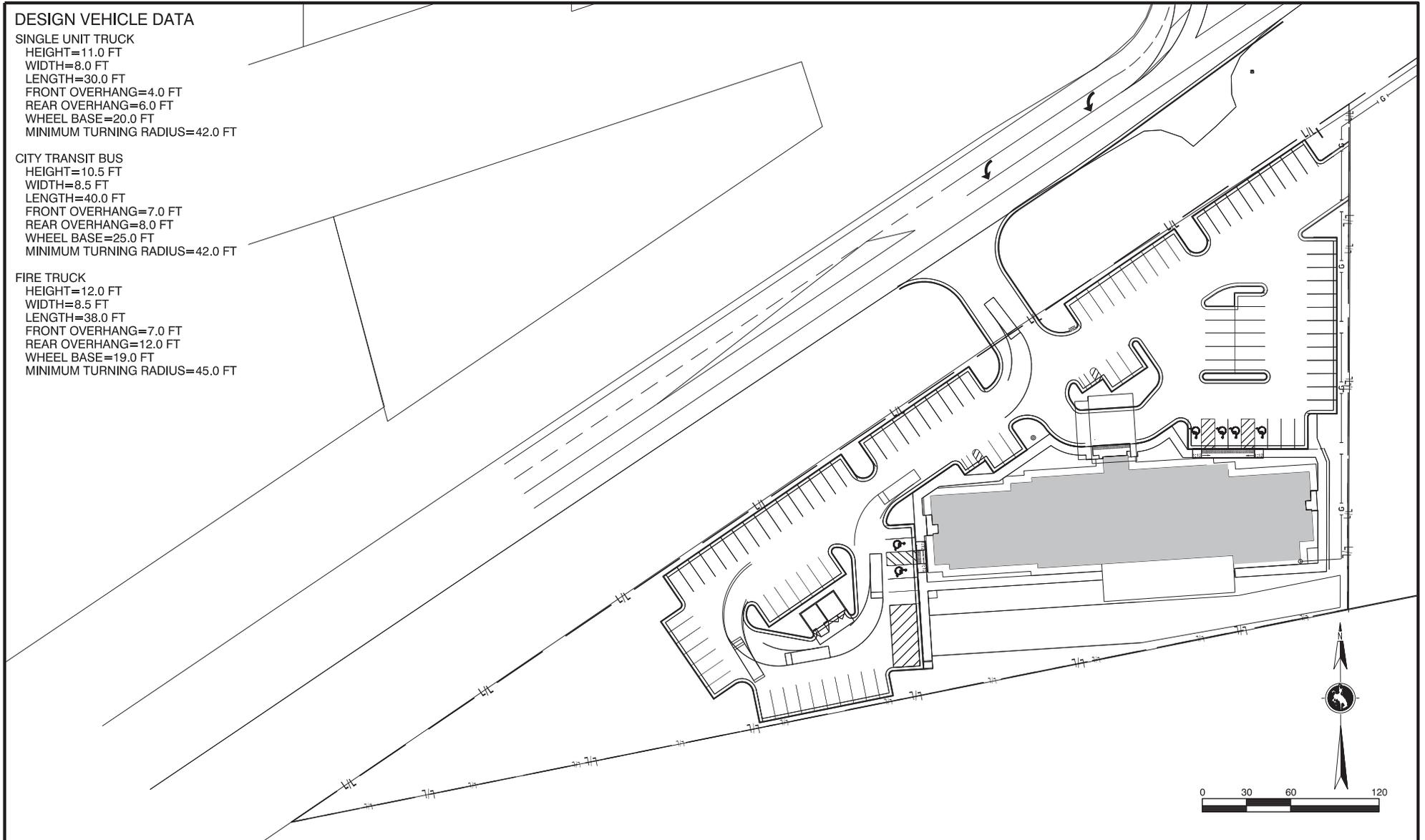


DESIGN VEHICLE DATA

SINGLE UNIT TRUCK
 HEIGHT=11.0 FT
 WIDTH=8.0 FT
 LENGTH=30.0 FT
 FRONT OVERHANG=4.0 FT
 REAR OVERHANG=6.0 FT
 WHEEL BASE=20.0 FT
 MINIMUM TURNING RADIUS=42.0 FT

CITY TRANSIT BUS
 HEIGHT=10.5 FT
 WIDTH=8.5 FT
 LENGTH=40.0 FT
 FRONT OVERHANG=7.0 FT
 REAR OVERHANG=8.0 FT
 WHEEL BASE=25.0 FT
 MINIMUM TURNING RADIUS=42.0 FT

FIRE TRUCK
 HEIGHT=12.0 FT
 WIDTH=8.5 FT
 LENGTH=38.0 FT
 FRONT OVERHANG=7.0 FT
 REAR OVERHANG=12.0 FT
 WHEEL BASE=19.0 FT
 MINIMUM TURNING RADIUS=45.0 FT



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SINGLE UNIT TRUCK TURNING PATHS

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 JPK TR CODY, LLC**

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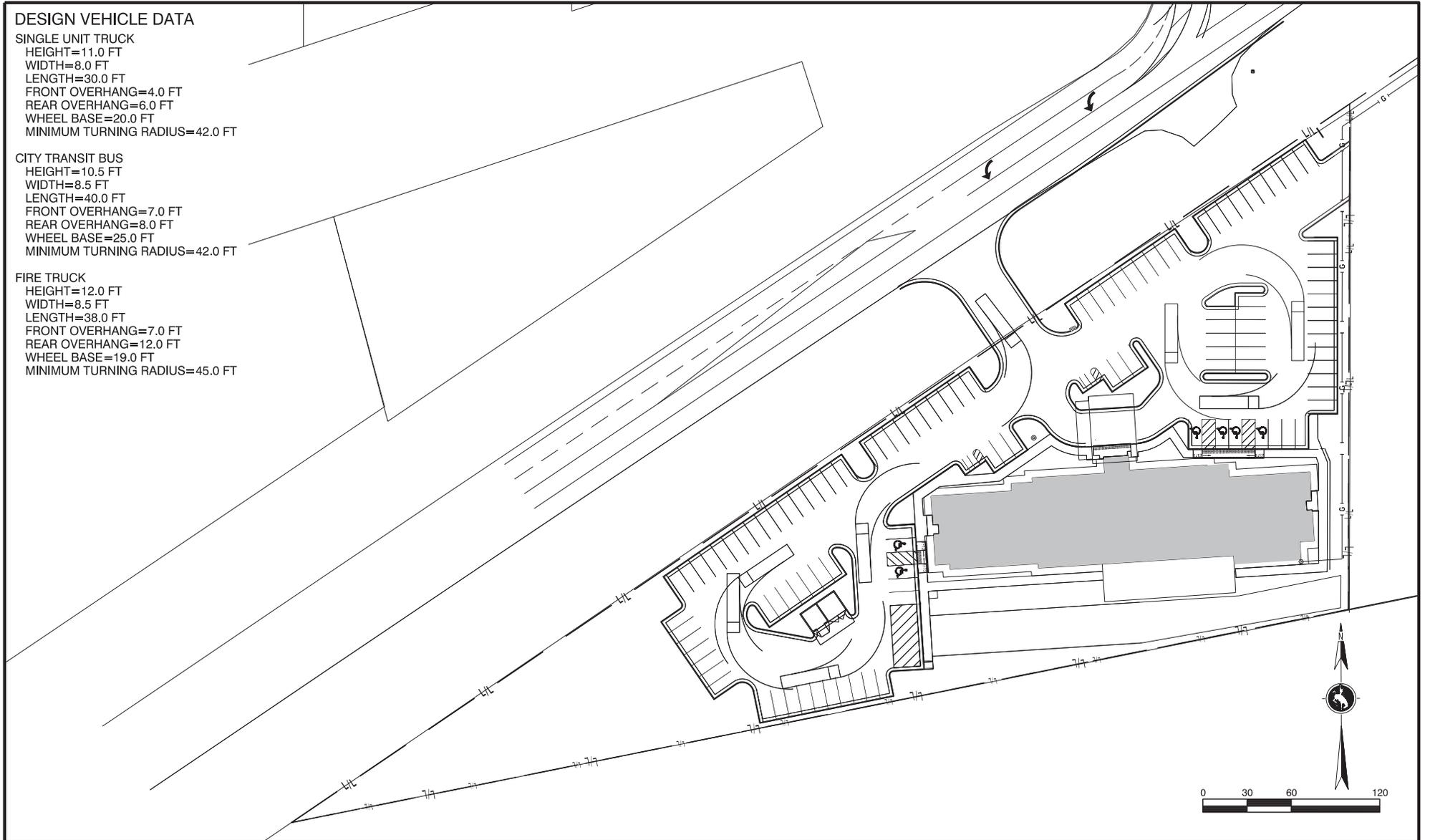


DESIGN VEHICLE DATA

SINGLE UNIT TRUCK
 HEIGHT=11.0 FT
 WIDTH=8.0 FT
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 FRONT OVERHANG=7.0 FT
 REAR OVERHANG=12.0 FT
 WHEEL BASE=19.0 FT
 MINIMUM TURNING RADIUS=45.0 FT



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08/30/2019	REVISED P&Z SUBMITTAL	CHECKED BY: TSW
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CITY BUS TURNING PATHS

**HAMPTON INN & SUITES
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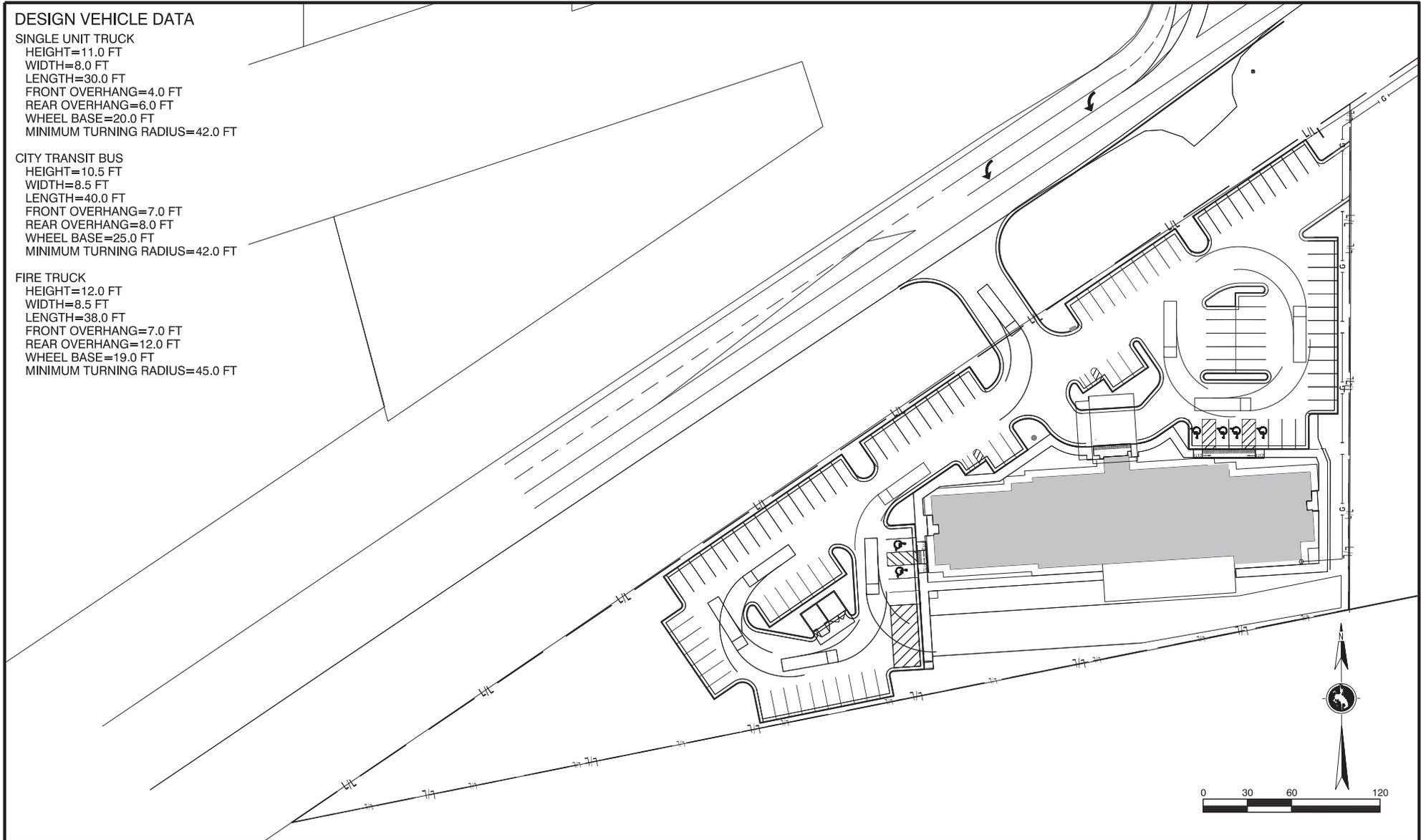


DESIGN VEHICLE DATA

SINGLE UNIT TRUCK
 HEIGHT=11.0 FT
 WIDTH=8.0 FT
 LENGTH=30.0 FT
 FRONT OVERHANG=4.0 FT
 REAR OVERHANG=6.0 FT
 WHEEL BASE=20.0 FT
 MINIMUM TURNING RADIUS=42.0 FT

CITY TRANSIT BUS
 HEIGHT=10.5 FT
 WIDTH=8.5 FT
 LENGTH=40.0 FT
 FRONT OVERHANG=7.0 FT
 REAR OVERHANG=8.0 FT
 WHEEL BASE=25.0 FT
 MINIMUM TURNING RADIUS=42.0 FT

FIRE TRUCK
 HEIGHT=12.0 FT
 WIDTH=8.5 FT
 LENGTH=38.0 FT
 FRONT OVERHANG=7.0 FT
 REAR OVERHANG=12.0 FT
 WHEEL BASE=19.0 FT
 MINIMUM TURNING RADIUS=45.0 FT



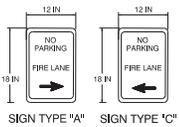
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FIRE TRUCK TURNING PATHS

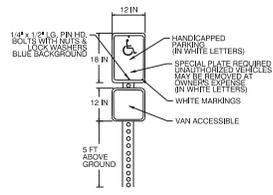
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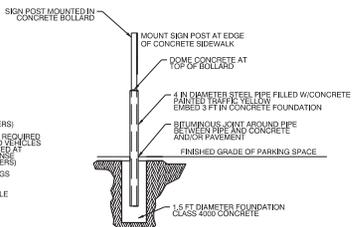




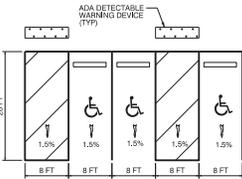
FIRE LANE SIGN DETAILS



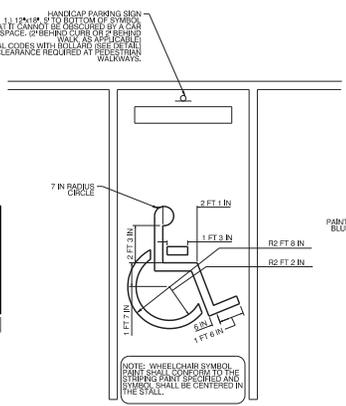
HANDICAP PARKING SIGN DETAIL



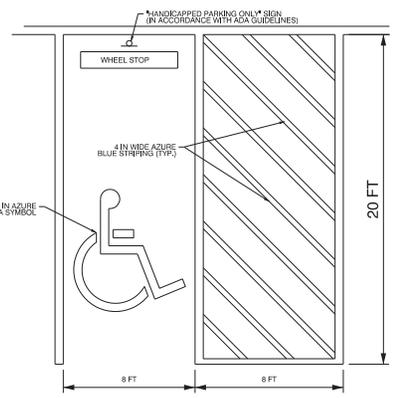
HANDICAP PARKING SIGN BOLLARD DETAIL



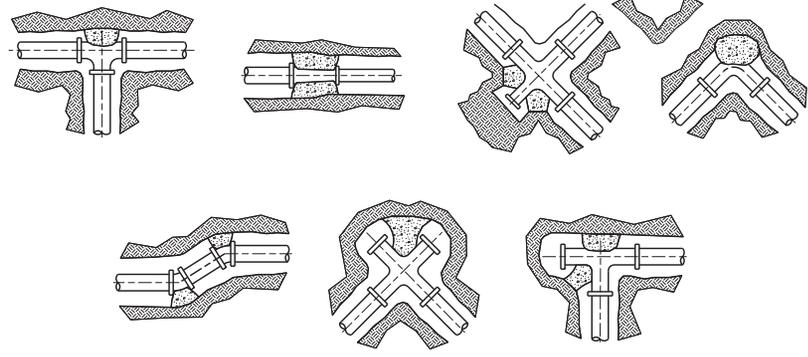
ADA PARKING DETAILS



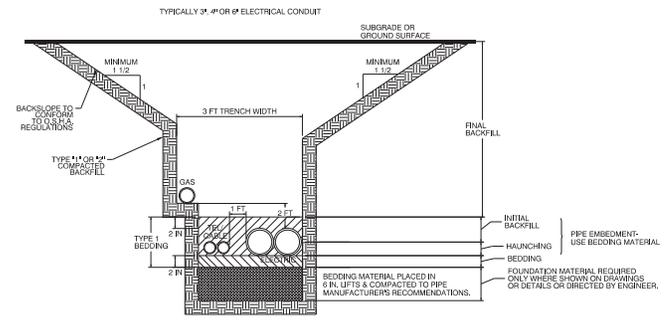
ADA STRIPING DETAILS



NOTE: ALL PAVEMENT STRIPING AND MARKINGS SHALL BE PAINTED WITH A DURABLE RUBBER PAINT. THE TEMPERATURE OF THE PAINT SHALL BE GREATER THAN 40°F. PAINT SHALL BE APPLIED AT A MINIMUM OF 0.015 IN MIL THICKNESS.



CONCRETE THRUST BLOCKS



TYPICAL TRENCH DETAIL - ELECTRICAL

IF SUPPORT OR SHIELD SYSTEM IS NOT USED, MAXIMUM ALLOWABLE SLOPE FOR ALL TRENCHES LESS THAN 6 FT. DEEP WILL BE 1:0.5V. COMPLY WITH APPLICABLE SAFETY REQUIREMENTS.

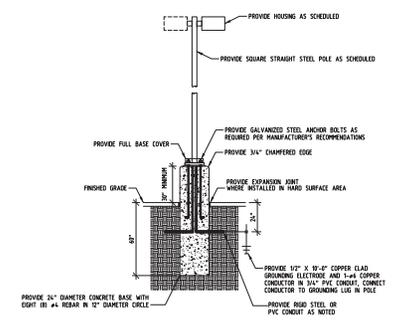
DATE	REVISION	DESCRIPTION	JOB NO.
08/30/2019	REVISED P&Z SUBMITTAL		2017-17
		DRAWN BY: TSW	
		CHECKED BY: TSW	
		APPROVED BY: DNS	

DETAILS

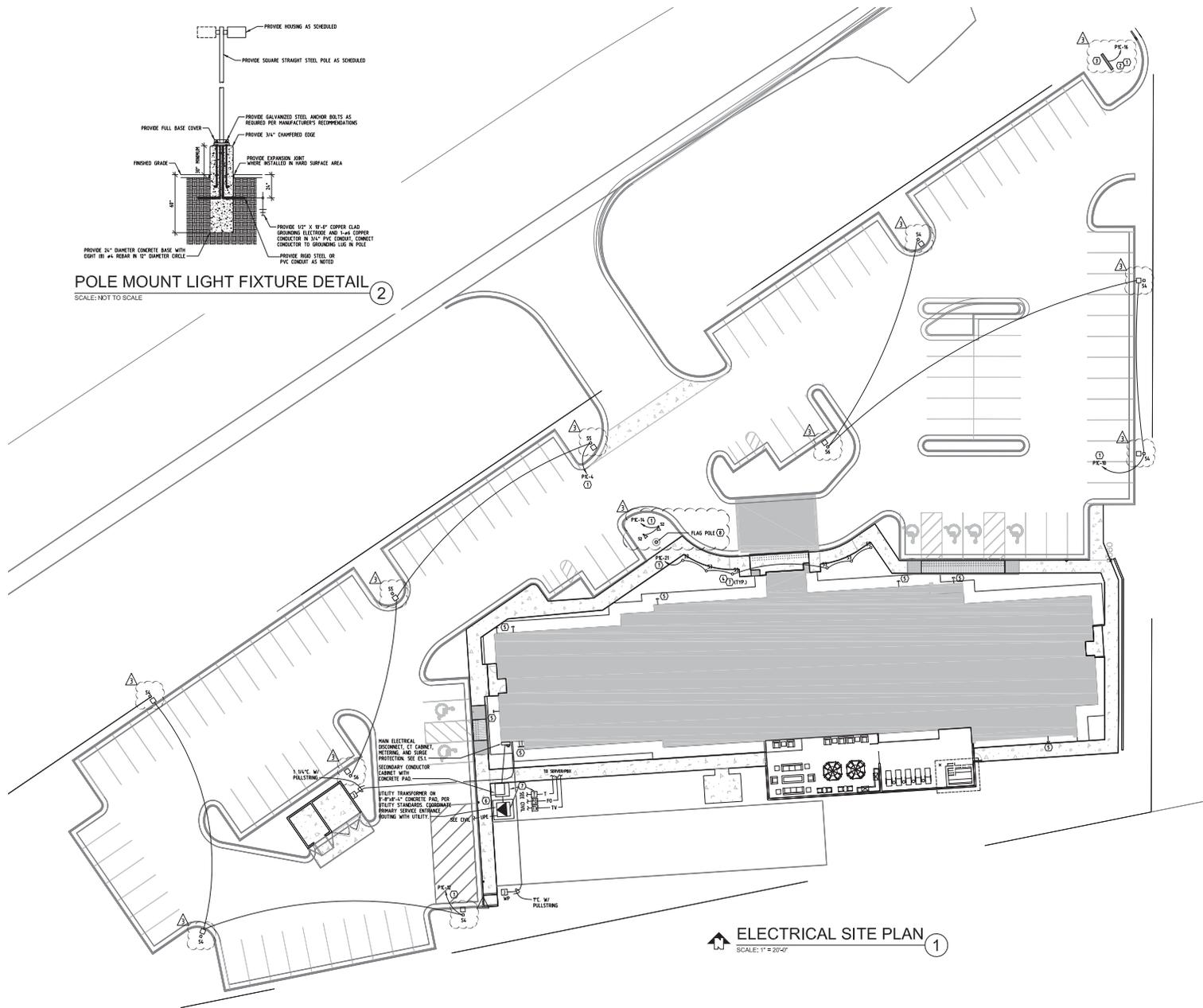
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CODY, WY 82414
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POLE MOUNT LIGHT FIXTURE DETAIL
SCALE: NOT TO SCALE



ELECTRICAL SITE PLAN
SCALE: 1" = 20'-0"

SHEET NOTES

POINT OF CONTACT KEY NOTE SYMBOL = (K)

COMPANY: NORTHWESTERN ENERGY
ADDRESS: 101 EAST 69TH DR, BOZEMAN, MONTANA 59715
NAME: BRIGMAN LYON
PHONE: (406) 271-2920
EMAIL: BRIGMAN@NORTHWESTERNENERGY.COM

GENERAL NOTES

- A. ALL WORK SHALL COMPLY WITH THE APPLICABLE LAWS AND REGULATIONS OF THE STATE OF MONTANA, WITH ALL REQUIREMENTS OF THE LATEST EDITION OF THE NATIONAL ELECTRICAL CODE (NEC), WITH THE EXCEPTION OF THE NATIONAL ELECTRICAL CODE (NEC) WHICH SHALL BE THE 2017 EDITION. IN THE EVENT OF CONFLICT BETWEEN THESE PLANS AND GOVERNING LAWS, THE MORE STRICT SHALL PREVAIL.
- B. THESE PLANS ARE SCHEMATIC IN NATURE. THROUGH A LOT OF DETAILS MAY BE SHOWN, THEY ARE NOT INTENDED TO SHOW EVERY DETAIL. THE INTENT IS TO PERFORM A GENERAL SCHEME OF WORK. THE CONTRACTOR SHALL VISIT JOB SITE PRIOR TO BIDDING TO SEE ACTUAL CONDITIONS.
- C. COMPLIANCE WITH THE WYOMING DE LAW REQUIRES THAT ALL PERSONS PRIOR TO BEING ANY TYPE OF TRUCK DRIVER CONTACT THE "WYOMING 811" NUMBER 800-545-2476. THIS SHOULD BE DONE A MINIMUM OF 48 HOURS BEFORE ANY EXCAVATION, SHEDDING AND RELOADS, PRIOR TO ACTUAL NEED TO ARRANGE FOR LOCATION OF UNDERGROUND UTILITY CABLES AND EQUIPMENT. ACTUAL LOCATING OF THE CABLES AND EQUIPMENT WILL THEN BE DONE BY FACILITY UTILITY. GENERAL SERVICE OR RESIDENTIAL CUSTOMER MUST MAKE SEPARATE ARRANGEMENTS FOR LOCATION OF THEIR NON-UTILITY-COVERED UNDERGROUND FACILITIES SITUATED WITHIN THEIR OWN PROPERTY.
- D. UTILITY LOCATIONS ARE SUBJECT TO INTERPRETATION. LOCATIONS ARE APPROXIMATE AND NO GUARANTEE IS MADE OR IMPLIED AS TO THEIR ACCURACY. FURTHER PRECAUTION MAY BE REQUIRED TO IDENTIFY UTILITIES. CONTACT LOCAL UTILITY COMPANIES FOR REQUIREMENTS. COORDINATION ON CHANGING AND/OR CANCELLING MUST BE DONE PRIOR TO INSTALLATION. ALL INSTALLATIONS MUST BE INSTALLED PER LOCAL UTILITY STANDARDS AND INSPECTED PRIOR TO BEING COVERED.
- E. CONTRACTOR SHALL COVER ALL COSTS FOR CLEARING OTHER OPENINGS OR UNDERGROUND ROUTES, INCLUDING THE REMOVAL, BUILDING AND/OR FOUNDATION OR RUBBLE REMOVAL AND ANY OTHER OBSTACLES ENCOUNTERED.
- F. REFER TO ELECTRICAL/TELECOM RISER DIAGRAMS FOR ADDITIONAL INFORMATION. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SITE WORK REQUIRED BY LOCAL UTILITIES.
- G. CONTRACTOR SHALL COORDINATE ALL REQUIREMENTS WITH LOCAL COMPARTMENTED READING ROOMS/CONTRACTOR COST. AND TO THE CONTRACTOR COST SHALL BE INCLUDED AS ALLOWANCE AND NOT PART OF THE CONTRACTOR BID.
- H. UTILITIES SHOWN ARE PRELIMINARY. COORDINATE WITH CIVIL AND UTILITIES EXACT LOCATIONS AND ALL REQUIREMENTS.
- I. PROVIDE EXTERNAL GLASS SHIELDS FOR PARKING LOT POLE LIGHTS IF REQUIRED BY THE CITY OF CODY FOR GLARE ISSUES ON SOUTH PARK ROAD.

KEY NOTES

1. CONNECT THRU LIGHTING CONTROL PANEL. PROVIDE 2-1/2" MIN. Ø THROUGHOUT.
2. CONNECT THROUGH STUB-OUT (SEE SHEET E10)
3. MOUNTING SIGN VERY EXACT LOCATION WITH ARCHITECT AND CIVIL.
4. COORDINATE BALLARD LOCATIONS WITH ARCHITECT/OWNER.
5. SEE STUB-OUTS. SEE CIVIL AND SHEETS E11 AND E12 FOR MORE INFORMATION.
6. PROVIDE STEEL REINFORCED IF THESE CONCRETE HOUSING/KEEPING PAD WITH 1/2" Ø 10" DIAMETER CONCRETE SUPPORT COLLUMS DOWN TO BELOW FROST DEPTH.
7. COVER EXACT LOCATION WITH CIVIL ARCHITECT.
8. FLAGPOLE BY OTHERS. COORDINATE INSTALLATION OF LIGHTS.

JOB NUMBER	17041
ISSUE DATE	11-09-18
REVISIONS	
ADD 01	01/18/19
ADD 02	02/27/19
ADD 03	08/15/19



Hampton Inn & Suites
8 SOUTH FORK ROAD
CODY, WYOMING

ARCHITECTURE | PLANNING | INTERIOR DESIGN | PROCUREMENT
CENTRAL DESIGN GROUP II • 1402 HATHMAN PL., SUITE 100 • COLUMBIA, MISSOURI 65201 • TEL: 573-441-9391

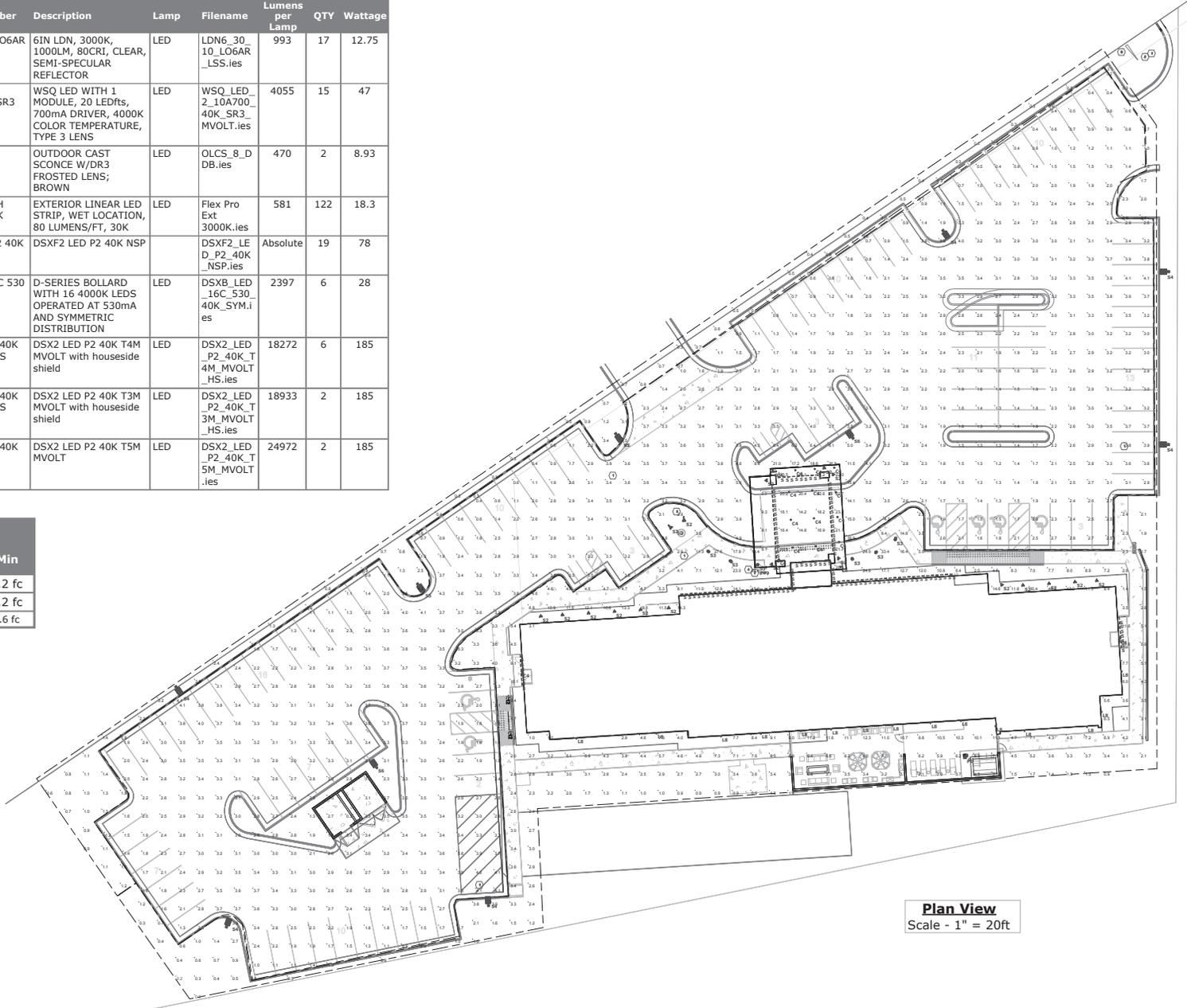


SITE PLAN - ELECTRICAL
E1.0

AES ADVANCED ENGINEERING SYSTEMS
www.aes-inc.com PROJECT # 18-127
4630 ANTELOPE CREEK RD STE 200 P: 402-688-0075
LINCOLN, NE 68506 F: 402-688-0272

LLF	Symbol	Label	Manufacturer	Catalog Number	Description	Lamp	Filename	Lumens per Lamp	QTY	Wattage
	○	C4	Lithonia Lighting	LDN6 30/10 LO6AR LSS	6IN LDN, 3000K, 1000LM, 80CRI, CLEAR, SEMI-SPECULAR REFLECTOR	LED	LDN6_30_10_LO6AR_LSS.ies	993	17	12.75
	○	L8	Lithonia Lighting	WSQ LED 2 10A700/40K SR3 MVOLT	WSQ LED WITH 1 MODULE, 20 LEDfts, 700mA DRIVER, 4000K COLOR TEMPERATURE, TYPE 3 LENS	LED	WSQ_LED_2_10A700_40K_SR3_MVOLT.ies	4055	15	47
	□	P1	Lithonia Lighting	OLCS 8 DDB	OUTDOOR CAST SCONCE W/DR3 FROSTED LENS; BROWN	LED	OLCS_8_D DB.ies	470	2	8.93
		S	ACCLAIM LIGHTING	FLEXPE3 WITH FLKXL TRACK	EXTERIOR LINEAR LED STRIP, WET LOCATION, 80 LUMENS/FT, 30K	LED	Flex Pro Ext 3000K.ies	581	122	18.3
	△	S2	Lithonia Lighting	DSXF2 LED P2 40K NSP	DSXF2 LED P2 40K NSP		DSXF2_LE D_P2_40K_NSP.ies	Absolute	19	78
	○	S3	Lithonia Lighting	DSXB LED 16C 530 40K SYM	D-SERIES BOLLARD WITH 16 4000K LEDS OPERATED AT 530mA AND SYMMETRIC DISTRIBUTION	LED	DSXB_LED_16C_530_40K_SYM.ies	2397	6	28
	□	S4	Lithonia Lighting	DSX2 LED P2 40K T4M MVOLT HS	DSX2 LED P2 40K T4M MVOLT with houseside shield	LED	DSX2_LED_P2_40K_T4M_MVOLT_HS.ies	18272	6	185
	□	S5	Lithonia Lighting	DSX2 LED P2 40K T3M MVOLT HS	DSX2 LED P2 40K T3M MVOLT with houseside shield	LED	DSX2_LED_P2_40K_T3M_MVOLT_HS.ies	18933	2	185
	□	S6	Lithonia Lighting	DSX2 LED P2 40K T5M MVOLT	DSX2 LED P2 40K T5M MVOLT	LED	DSX2_LED_P2_40K_T5M_MVOLT.ies	24972	2	185

Statistics			
Description	Avg	Max	Min
Landscaping	4.8 fc	48.3 fc	0.2 fc
Parking Lot	2.8 fc	26.5 fc	0.2 fc
Porte Cochere	18.5 fc	26.3 fc	8.6 fc



Plan View
Scale - 1" = 20ft



Mark Gordon
Governor

WYOMING Department of Transportation

"Providing a safe, high quality, and efficient transportation system"

P.O. Box 461, Basin, Wyoming 82410



K. Luke Reiner
Director

August 9, 2019

Mr. John Koehler
JPK TR Cody, LLC
P.O. Box 15
Aberdeen, SD 57402

RE: Proposed Hampton Inn Access, Wyoming State Highway 291

Dear Mr. Koehler,

This letter is to inform you that your latest proposal dated June 26, 2019 for a new commercial access on Wyoming State Highway 291 (Southfork Road) in Cody, WY is acceptable in principle to the Wyoming Department of Transportation (WYDOT). Prior to issuing an access permit, WYDOT will need to review final design plans for construction work being done in the WYDOT right-of-way. The plans should include proposed access geometry, earthwork grading, surfacing, curb and gutter, sidewalk, guardrail modifications and striping layouts. In addition, a length of need analysis for the guardrail modification shall also be submitted.

Please submit a new access application that reflects the agreed upon location and striping modifications. WYDOT forces will make the striping modifications on Wyoming 291 at your expense. You will need to enter into an Authority for Rendering Special Services (ARS) agreement with WYDOT prior to striping modifications. I will provide you with an estimate for the striping once final striping layouts are received.

This access approval will be for the proposed Hampton Inn only. If the lot to the east of the Hampton Inn site is developed WYDOT will require a new access application to review the added traffic impacts.

If you would like to further discuss this, please feel free to call me at (307) 568-3400.

Sincerely,

Jack T. Hoffman, P.E.
District Traffic Engineer, Basin

cc: Shelby Carlson, P.E., Chief Engineer, Cheyenne
Mark Gillett, P.E., Assistant Chief Engineer-Operations, Cheyenne
Pete Hallsten, P.E., District Engineer, Basin
Joel Meena, P.E., State Traffic Engineer, Cheyenne
Phillip Bowman, P.E., C.F.M., Public Works Director, City of Cody
Todd Stowell, A.I.C.P., Community Development Director, City of Cody

DRAINAGE REPORT

For

Hampton Inn & Suites
8 Southfork Road – Cody, WY

Owner
JPK TR Cody

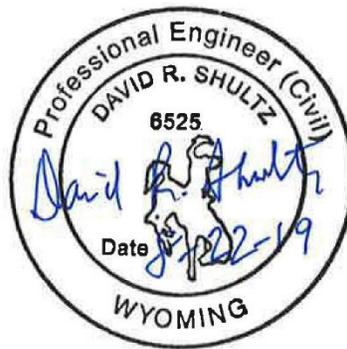
P.O. Box 15
Aberdeen, SD 57402

Engineer



SAGE CIVIL ENGINEERING
ENGINEERS | SURVEYORS

2824 Big Horn Ave.
Cody, WY 82414



January 17, 2019
Revised August 22, 2019

Introduction - Property Description

This project location is southwest of the intersection of WY 14/16/20 and Southfork Road (Hwy. 291) within lot 71 and 72, Resurvey T.52N., R.102W., 6th P.M. Park County, Wyoming, and consists of 2.82 acres/122,840 ft².

The site is zoned D-3 but was previously a residence. All buildings have been removed leaving a vacant lot and ground cover of predominantly gravel and natural grass and sagebrush. The existing terrain slopes generally from southwest to northeast, with historical runoff collecting on the property to the east of the subject lot and to the highway right-of-way. The subject property was recently split from the property to the east and was previously owned by a single entity.



Photo 1 - Existing Conditions

Purpose of Drainage Plan

This drainage plan outlines the proposed measures to handle storm water runoff for this development. Drainage calculations have been performed and subsequent storm water facilities will be constructed as shown on the drainage plan.

Description of Basins and Facilities

Proposed for the site is a 4-story, 92-room hotel. Surrounding the hotel will be sidewalks and landscaping, paved parking, drainage facilities, and a fire access to be surfaced with clean crushed rock.

Post-development configuration of the property will result in two separate drainage basins.

Drainage Basin 1 includes a small area east of the building and parking area. Improvements in this area will include landscaping, paving, and sidewalk. The runoff from this area will amount to only about five percent of the historic volume from this property, and will continue to flow to the north and east to the historic collection points.

Drainage Basin 2 includes the remainder of the property, including drains from the hotel roof and the pools (swimming and hot tub) and a patio area on the back of the building. The volume of the pools was not included in the design calculations as draining of these facilities will not be performed during a design storm event. Storm water will drain generally away from the building in the north, south, and west directions to numerous storm inlets and then conveyed by pipe to the single percolation trench in the southwest corner of the property.

SCE has performed percolation tests throughout the city and designed numerous drainage facilities for developments in this area based on these tests, and the facilities all function satisfactorily. Percolation rates for the underlying gravel have been found to range from $\frac{3}{8}$ inch per minute to greater than $\frac{3}{4}$ inch per minute. Percolation trenches planned for this project will be constructed providing drainage directly into the cobble soils, and a conservative percolation rate of $\frac{1}{4}$ inch per minute was used for calculations.

Percolation Trench Maintenance: Cobble will be cleaned or replaced if void spaces become filled with sediment or other debris to the point the facilities become ineffective.

Calculations

The storm drainage runoff calculations were based on the following data:

Applicable C values are:

C _{Pavement/Concrete/Building}	= 0.90
C _{Compacted Gravel}	= 0.60
C _{Landscaping/Unimproved/Percolation Areas}	= 0.30

Existing Conditions: As mentioned in the Introduction, all runoff appears to collect on the area east of the subject property. Therefore, the Existing Conditions runoff calculation is considered the amount of historical runoff to leave the property. The existing ground cover consists of compacted gravel and unimproved (natural) vegetation.

Unimproved	= 84,900 sf
Compacted Gravel	= 37,940 sf

Post-Development Conditions:

Drainage Basin 1

Landscaped Areas	= 1,118 sf
Pavement	= 4,912 sf

Drainage Basin 2

Landscaping/Percolation Areas	= 37,342 sf
Compacted Gravel (Fire Lane)	= 7,372 sf
Pavement/Sidewalk	= 56,951 sf
Building Roof Area	= 15,145 sf

Per the City's SWMP, section 3.6.4, the design storm is to be based on a 25-year, 2-hour event for percolation trenches, therefore the Intensity (I) = 0.66 in/hr. Storm volume calculations are shown beginning on the next page.

EXISTING CONDITIONS	Area	C x A	Q	V 2hrs
	(ft ²)	(ac)	(cfs)	(ft ³)
Unimproved (C=0.30)	84900	0.5847	0.386	2779
Compacted Gravel (C=0.60)	37940	0.7839	0.517	3725
Total			0.903	6504

POST-DEVELOPMENT CONDITIONS	Area	C x A	Q	V 2hrs
	(ft ²)	(ac)	(cfs)	(ft ³)
Drainage Basin 1				
Landscape/Perc Trench (C=0.30)	4241	0.0292	0.019	139
Pavement/Sidewalk (C=0.90)	1789	0.0370	0.024	176
Basin 1 Total			0.044	314

POST-DEVELOPMENT CONDITIONS	Area	C x A	Q	V 2hrs
	(ft ²)	(ac)	(cfs)	(ft ³)
Drainage Basin 2				
Landscape/Perc Trench (C=0.30)	37342	0.2572	0.170	1222
Compacted Gravel (C=0.60)	7372	0.1015	0.067	483
Pavement/Sidewalk (C=0.90)	56951	1.1767	0.777	5592
Building Roof Area (C=0.90)	15145	0.3129	0.207	1487
Basin 2 Total			1.220	8783

The total volume of water required to be disposed is the volume from Drainage Basin 2, which equals 8,783 ft³.

Percolation Trench Summary

A geotechnical investigation was performed on the property that indicates the gravel/cobble layer that is typical of this area and which provides excellent percolation is generally covered with approximately one foot of topsoil. The percolation area to which all collected runoff will be directed will be created by excavating material to be used as embankment for the site. The table below illustrates the volume of runoff that would be disposed by percolation-only. The percolation area used in the calculations is conservative and is equal to the surface area in the pit that is below the pipe outlet elevation. Although no credit is taken for storage volume in the summary table below, the volume of the perc area after excavation is expected to provide 40,000 to 45,000 cubic feet of storage volume.

	Percolation Area	Storage Volume	Percolation Rate		Volume Percolated During Storm	Total Volume Disposed
	(ft ²)	(ft ³)	(in/minute)	(ft/sec)	(ft ³)	(ft ³)
Drainage Basin 1						
Historic Flow Area						0
Drainage Basin 2						
Southwest Perc Area	4667	0	0.25	0.000347	11668	11668
Total Volume Disposed						11668



Department of Environmental Quality

To protect, conserve, and enhance the Quality of Wyoming's Environment for the benefit of current and future generations



Mark Gordon, Governor

Todd Parfitt, Director

NOTIFICATION OF COVERAGE

May 13, 2019

John Koehler, Manager
JPK TR Cody, LLC
P.O. Box 15
Aberdeen, SD 57402

RE: Cody – Hampton Inn & Suites Water and Sewer Project, **Permit No. 19-140, Park County**

Dear Mr. Koehler:

The Department of Environmental Quality (DEQ) has reviewed and approved the above application for coverage under the Sewage Collection Systems and Public Water Supply Distribution General Permits in accordance with Chapter 3, Section 7 of the Wyoming Water Quality Rules and Regulations (WQRR) and hereby issues this Notice of Coverage (NOC). The facilities are located at T52N, R102W (**44.515 Lat / -109.096 Long**)

This NOC involves installing approximately 636 linear feet of 8-inch and 6-inch water main with one fire hydrant and 615 linear feet of 6-inch sewer main with three new manholes to serve 92-room hotel.

DEQ authorizes you to construct, install, or modify the facility in accordance with **Chapter WQRR Chapters 11 and 12**, the general permit, and the materials submitted in your application package. Please note Part V, Standard Permit Conditions, of the general permit, particularly the Right to Access and Reporting Requirements sections. A copy of the general permit is available on the DEQ webpage: <http://deq.wyoming.gov/wqd/permitting-2/resources/general-permits-2/>.

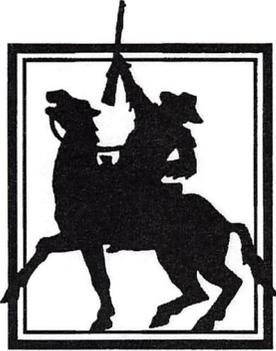
DEQ requires that all construction, installation, or modifications allowed by this NOC shall be completed by **May 31, 2024**. If you have any questions, please contact James Brough at 307-335-6961 or james.brough@wyo.gov.

Sincerely,

James Brough, P.E.
Northwest District Engineer
WDEQ/WQD

cc: IPS, Cheyenne (pdf)
Phillip Bowman, P.E., City of Cody, P.O. Box 2200, Cody, WY 82414
David Shultz, Sage Civil Engineering, 2824 Big Horn Ave., Cody, WY 82414





CITY OF CODY
WYOMING

Matt Hall
MAYOR

Justin Baily
Diane Ballard
Jerry Fritz
Landon Greer
Glenn A. Nielson
Heidi Rasmussen
COUNCIL MEMBERS

C. Edward Webster II
MUNICIPAL JUDGE

Barry A. Cook
CITY ADMINISTRATOR

1338 Rumsey Avenue
P.O. Box 2200
Cody, Wyoming 82414

(307) 527-7511
FAX (307) 527-6532

April 24, 2019

James S. Brough, P.E.
Northwest District Engineer
Water Quality Division - WDEQ
510 Meadowview Drive
Lander, WY 82520

Subject: Hampton Inn & Suites by JPK TR Cody, LLC, Cody, Wyoming

Dear Mr. Brough:

The City of Cody has reviewed the plans and design reports prepared for the proposed Hampton Inn & Suites (project) to be located at the intersection of Southfork Road (SH 291) and West Yellowstone Ave (US 14/16/20) within the incorporated city limits of the City of Cody, Wyoming.

The City of Cody has reviewed the proposed treated water main and sanitary sewer construction plans for the project and found these plans to address all local concerns. It has been agreed by the City, the developer, and the design engineer that the proposed 8" treated water main will be a private water service main, and that City maintenance will end at the proposed 8" valve to be included with the tee and valve cluster at STA 20+00. It was further agreed by all parties that the fire hydrant shown at STA 25+65.35 will be considered private (not maintained by the City), but will be available for any fire district or fire-fighting agency to use on this site, the adjacent property(ies), and/or adjacent Right-of-Way should the need arise.

In addition, it has also been agreed by the City, the developer, and the design engineer that the proposed 6" sanitary sewer main will be a private sewer service main, and that City maintenance will end at the existing sanitary sewer manhole and sanitary sewer main connection at STA 10+00.

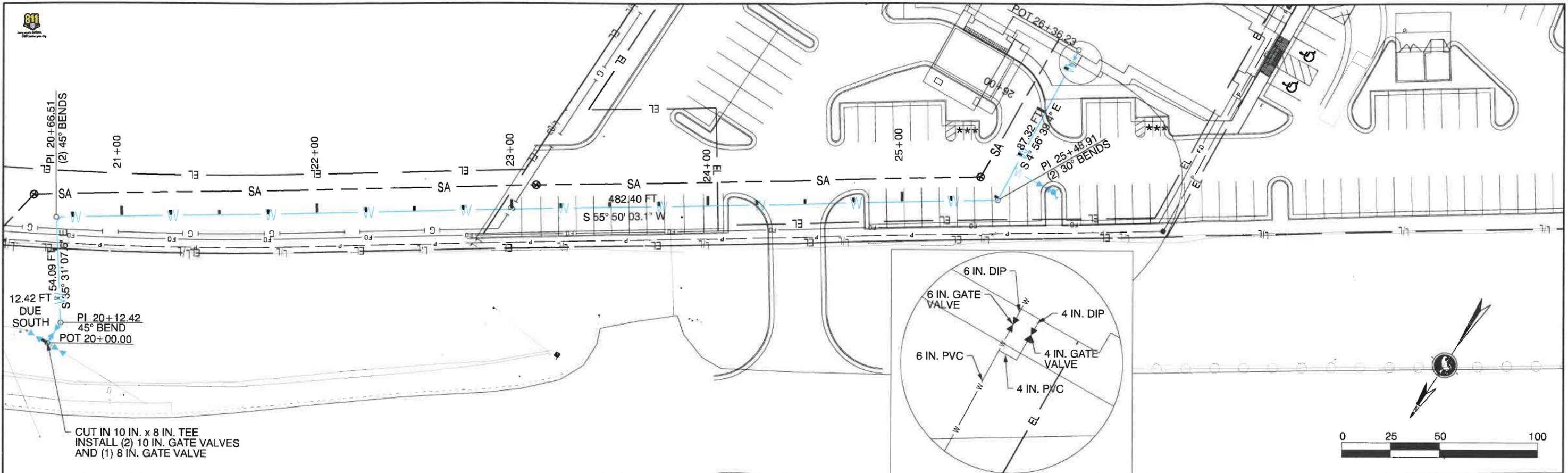
Based on the review of the design information provided by Sage Civil Engineering, the City's treated water distribution system, wastewater collection system, and wastewater treatment system all have sufficient capacity to serve the proposed Hampton Inn & Suites project. With your review and approval of the "Permit to Construct" for the treated water main and sanitary sewer main extensions for this project, I will continue to work with the applicant and their engineer to finalize and approve all civil construction plans for the proposed project.

Please let me know if you have any questions or concerns about this project, or the City's review of the treated water and sanitary sewer plans. Thanks in advance.

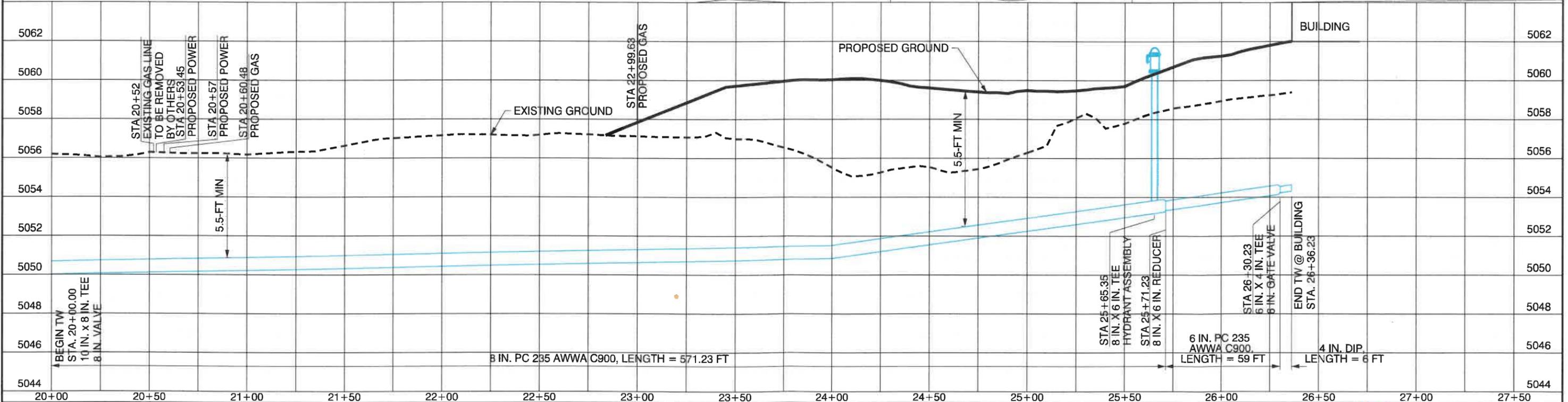
Sincerely,

Phillip M. Bowman, P.E., CFM
Public Works Director

Cc: Sage Civil Engineering
File



CUT IN 10 IN. x 8 IN. TEE
INSTALL (2) 10 IN. GATE VALVES
AND (1) 8 IN. GATE VALVE



REVISION	DESCRIPTION	FILE NO. - 2017-017
DATE	11/28/18 PRELIMINARY DRAWINGS	DRAWN BY - TSW
	01/18/19 P & Z REVISIONS	CHECKED BY - DRS
		APPROVED BY - DRS

TREATED WATER PLAN AND PROFILE

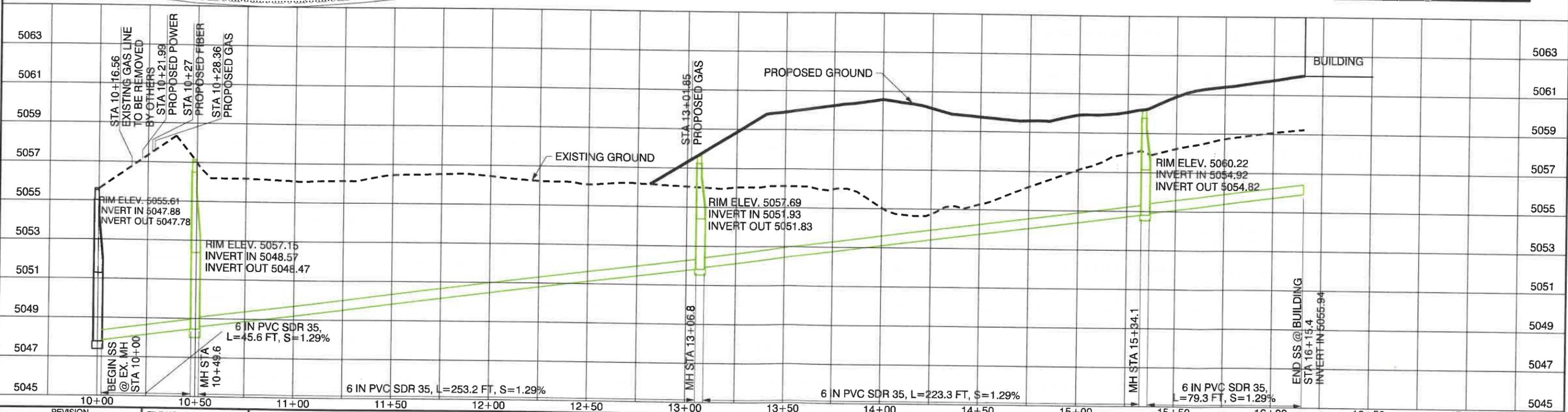
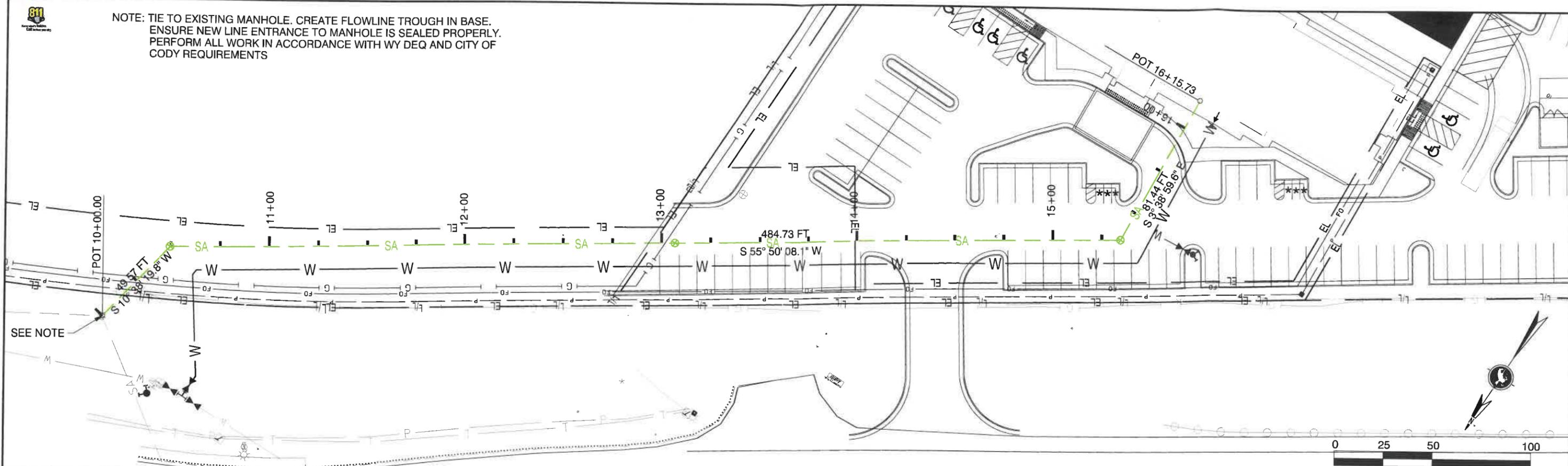
**HAMPTON INN & SUITES
JPK TR CODY, LLC**

2824 BIG HORN AVE.
CODY, WY 82414
PHONE: (307) 527-0915
WWW.SAGECIVILENGINEERING.COM

8 OF 9



NOTE: TIE TO EXISTING MANHOLE. CREATE FLOWLINE TROUGH IN BASE. ENSURE NEW LINE ENTRANCE TO MANHOLE IS SEALED PROPERLY. PERFORM ALL WORK IN ACCORDANCE WITH WY DEQ AND CITY OF CODY REQUIREMENTS



REVISION	DESCRIPTION	FILE NO. - 2017-017
11/28/18	PRELIMINARY DRAWINGS	DRAWN BY - TSW
01/18/19	P & Z REVISIONS	CHECKED BY - DRS
		APPROVED BY - DRS

SANITARY SEWER PLAN AND PROFILE

**HAMPTON INN & SUITES
JPK TR CODY, LLC**

2824 BIG HORN AVE.
CODY, WY 82414
PHONE: (307) 527-0915
WWW.SAGECIVILENGINEERING.COM





THE PROMISE AND CHALLENGE OF LED LIGHTING: A PRACTICAL GUIDE

A PUBLICATION OF THE
INTERNATIONAL DARK-SKY ASSOCIATION

The light-emitting diode (LED) is transforming the way we light our cities and towns, offering a once-in-a-lifetime chance to radically improve how we use energy and our outdoor spaces at night. With this opportunity comes an obligation to manage these changes responsibly and sustainably. The stakes are high and the potential rewards great, but outcomes depend critically on policymakers and the public having access to reliable information. IDA developed this document to provide planners, lighting designers and public officials an overview of the most important aspects of LED lighting and the choices and challenges involved in its municipal implementation.

What is LED?

LEDs use solid-state technology to convert electricity into light. Put simply, LEDs are very small light bulbs that fit into an electrical circuit. Unlike traditional incandescent bulbs, they don't have a filament that burns out and they don't get very warm. Initially, LEDs only emitted red, yellow, or green light, but now white LEDs are widely available. Early LEDs were also energy-inefficient and emitted little light, but due to technological advances LED efficiency and light output have doubled about every three years. Because of their improved quality and falling prices, LEDs are now replacing conventional high-intensity discharge (HID) lamp types for outdoor lighting in communities around the world.

Why Adopt This Technology?

The improved energy efficiency of LEDs means that, coupled with modern luminaire design, these lights allow for reduced illuminance without compromising safety. LEDs help lower carbon emissions by reducing the demand for electricity, which is still largely generated by burning fossil fuels. Another LED benefit is better control over the color content of light. Manufacturers now produce LEDs with "warm" color qualities at high energy efficiency, rendering old arguments about the perceived inefficiency of warm white LEDs moot. These same LED options also provide accurate color rendition without emitting excessive amounts of potentially harmful blue light (see below).

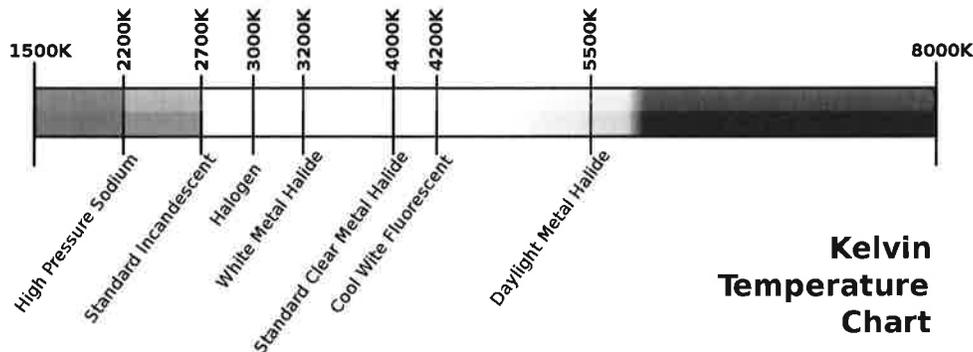
Relative to other outdoor lamps, LEDs are thought to be extremely long-lived. When switched on, LEDs are instantly at full brightness, unlike HID lamps that have a significant time delay to begin emitting light. LEDs also have very low minimum electricity thresholds to produce light, meaning they can be dimmed to much lower illumination levels when less light is needed and resulting in further energy savings.

Blue Light Is Bad

New technical capabilities often come with unanticipated challenges. Most white LED lighting has significant levels of potentially hazardous blue light. IDA published a report¹ in 2010 detailing the hazards of blue-rich white light sources. In the years since, scientific evidence has solidified around its conclusions. In June 2016, the American Medical Association (AMA) publicly concluded² that “white LED street lighting patterns [may] contribute to the risk of chronic disease in the populations of cities in which they have been installed.” The AMA recommends “minimizing and controlling blue-rich environmental lighting by using the lowest emission of blue light possible” in order to reduce potential negative effects on human health.

Concerns about blue light reach far beyond our health. Outdoor lighting with strong blue content is likely to worsen skyglow because it has a significantly larger geographic reach than lighting consisting of less blue. According to the 2016 “World Atlas of Artificial Night Sky Brightness” street lighting and outdoor lighting retrofits using 4000K lamps could result in a factor of 2.5 increase in light pollution.³ Given that the rate of increase of lighting as seen from Earth orbit is about 2 percent per year⁴, and much of the increase is attributable to white LED, it is all the more important to address this problem.

Blue-rich white light sources are also known to increase glare and compromise human vision, especially in the aging eye^{5,6}. These lights create potential road safety problems for motorists and pedestrians alike. In natural settings, blue light at night has been shown to adversely affect wildlife behavior and reproduction^{7,8}. This particularly true in cities, which are often stopover points for migratory species.



¹ <http://bit.ly/2gKiEfN>

² American Medical Association Council on Science and Public Health Report 2-A-16: “Human and Environmental Effects of Light Emitting Diode (LED) Community Lighting”, 2016 (PDF: <http://bit.ly/1UzSqVQ>)

³ Falchi et al., *Science Advances* (10 Jun 2016) Vol. 2, no. 6, e1600377. [10.1126/sciadv.1600377](https://doi.org/10.1126/sciadv.1600377)

⁴ Kyba et al., Artificially lit surface of Earth at night increasing in radiance and extent. *Science Advances* (22 Nov 2017) Vol. 3, no. 11, e1701528. [10.1126/sciadv.1701528](https://doi.org/10.1126/sciadv.1701528)

⁵ Lin et al. Model predicting discomfort glare caused by LED road lights. *Optics Express* (2014) Vol. 22, no. 15, 18056-71. [10.1364/OE.22.018056](https://doi.org/10.1364/OE.22.018056)

⁶ Sweater-Hickcox et al. Effect of different coloured luminous surrounds on LED discomfort glare perception. *Lighting Research Technology* (2013) Vol. 45, no. 4, 464-75. <http://lrt.sagepub.com/content/45/4/464>

⁷ Bennie et al. Ecological effects of artificial light at night on wild plants. *Journal of Ecology* (2016) Vol. 104, issue 3, 611-620. [10.1111/1365-2745.12551](https://doi.org/10.1111/1365-2745.12551)

⁸ Hori and Suzuki. Lethal effect of blue light on strawberry leaf beetle, *Galerucella griseascens* (Coleoptera: Chrysomelidae). *Scientific Reports* (2017) Article 2694. <https://www.nature.com/articles/s41598-017-03017-z>

The promise of cheaper outdoor lighting based on electricity and maintenance savings from LED conversion should be weighed against other factors, such as the blue light content of white LEDs. Blue-rich sources are the most efficient LEDs in terms of the conversion of electricity to light, and therefore have the lowest electricity cost to produce a given amount of light compared to “warmer,” less efficient white LED lamps. Every effort should be made to diminish or eliminate blue light exposure after dark.

Product Selection Considerations

Choosing LED products for outdoor lighting applications involves a series of considerations and tradeoffs. These include:

- **Luminous Efficiency** (Watts-to-lumens): How many lumens of light are produced per input Watt of electricity? More importantly, how many lumens from the light source are meeting the task (“Fixture Lumens” vs. “Lamp Lumens”)
- **Lumen Output**: How much light is produced relative to the amount required for a particular task? When replacing existing fixtures, it is important to use the only level of illumination needed, and not to adopt unneeded increases in brightness.
- **Correlated Color Temperature** (CCT): Does the light have a “warm” or “cool” quality?
- **Color Rendering Index** (CRI): How accurately does the light render colors to the human eye? A high CRI is not needed for all situations. The need for good color rendition should be considered relative to the lighting application in question.
- **Adaptive Control Integration**: Does the lighting make use of adaptive controls such as dimmers, timers, and/or motion sensors? These controls are the wave of the future in outdoor lighting and achieve additional energy savings, improve light source efficacy and increase visual task performance. It is important to build in the ability to make use of adaptive controls during the adoption of designs for new lighting installations, even if they will not immediately be implemented.
- **Heat Mitigation**: Is the lamp housing designed to adequately dissipate heat? Because LED efficiency decreases with rising operating temperature, controlling heat emitted by LED lamps is critical in warm climates.
- **Lumen Depreciation**: How robust is the lamp against efficiency loss over time? Manufacturers typically quote “L70,” the expected use time until a bulb reaches 70% of its initial light output.

Closely related to all these factors is expense: How much will LED replacement solutions cost? The price of commercial LED lighting products continues to drop, and capital cost recovery times for new LED street light installations, once 10 years or more, are now typically less than five years and continue to decline. As barriers to implementation fall, LED is gaining momentum as the lighting technology of choice in both new outdoor installations and existing replace-on-failure installations.

IDA Recommends

Already many white LED options are available on the outdoor lighting market and that number will only rise in the future. IDA has developed a set of recommendations for those choosing lighting systems. These suggestions will aid in the selection of lighting that is

energy and cost efficient, yet ensures safety and security, protects wildlife, and promotes the goal of dark night skies. These include:

- **Always choose fully shielded fixtures** that emit no light upward
- **Use “warm-white” or filtered LEDs** (CCT < 3000 K; S/P ratio < 1.2) to minimize blue emission
- **Look for products with adaptive controls** like dimmers, timers, and motion sensors
- **Consider dimming or turning off the lights during overnight hours**
- **Avoid the temptation to over-light** because of the higher luminous efficiency of LEDs.
- **Only light the exact space and in the amount required for particular tasks**

Learn more about outdoor lighting, blue light at night, and dark skies on the IDA website at www.darksky.org.

**CITY OF CODY
PLANNING, ZONING AND ADJUSTMENT BOARD
STAFF REPORT**

MEETING DATE:	SEPTEMBER 10, 2019	TYPE OF ACTION NEEDED	
AGENDA ITEM:		P&Z BOARD APPROVAL:	
SUBJECT:	PRELIMINARY PLAT OF THE BEACON HILL MINOR SUBDIVISION— A 3-LOT SUBDIVISION. SUB 2018-04	RECOMMENDATION TO COUNCIL:	X
PREPARED BY:	TODD STOWELL, CITY PLANNER	DISCUSSION ONLY:	

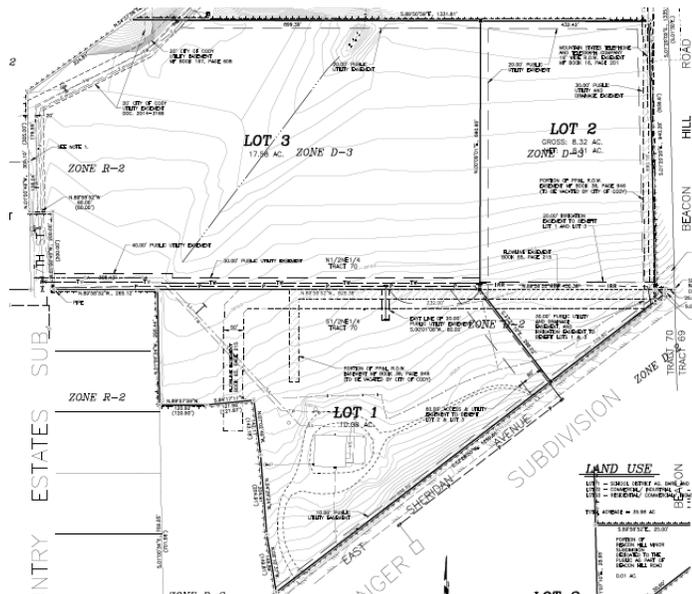
PROJECT OVERVIEW

The Planning and Zoning Board reviewed a preliminary plat for this property on May 14, 2019. The proposal has been revised and an amended preliminary plat submitted. The final plat is also submitted for review.

The property is owned by Park County School District No. 6 and involves the division of a 36-acre property northwest of the Sheridan Avenue and Beacon Hill Road intersection into three lots. Lot 1 contains the existing FFA barn and is anticipated to contain a new bus storage facility for School District #6. Lots 2 and 3 are currently vacant and are planned to be sold. The revised preliminary plat drawing and the final plat drawing are attached.



The primary change in the proposal was to enlarge the size of Lot 3 by about four acres, decrease the size of Lot 2 by about 3 ½ acres, and decrease the size of Lot 1 by about ½ acre. Doing so reduces the amount of off-site street improvements for Lot 3 and expands the development potential of Lot 3 to spread that cost over a larger development. Other changes address conditions or items noted in the prior review, so that the application is now relatively “clean”.

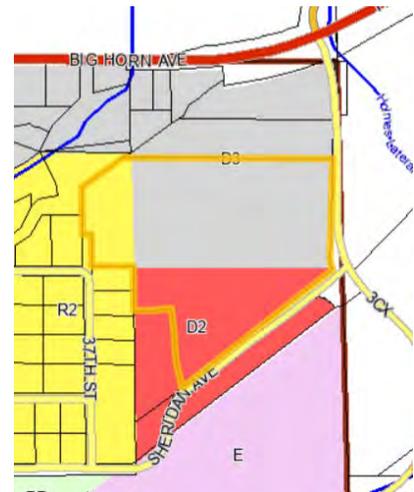


The property is located in three different zoning districts—R-2 (yellow), D-2 (red) and D-3 (gray), as shown on the map below. It is noted that the commercial zones (D-2 and D-3) also allow all forms of residential development. As Lots 2 and 3 are planned to be sold off, it is simply unknown how they may eventually be developed. With Lot 2 being over 8 acres in size and Lot 3 over 17 acres, they both have the potential for substantial development. Improvements for this subdivision are being planned and required based on a relatively moderate level of development on Lots 2 and 3.

As the preliminary plat was previously reviewed in a similar configuration, this staff report will only point out items that are not clearly reflected on the plats themselves, or are out of the ordinary.

Timing of Engineering and Responsibility for Improvements:

Depending on the level of future development there may be additional infrastructure improvements needed, such as looping the water main, and more substantial street improvements. However, to avoid both “overdesigning” and “overbuilding” the infrastructure, the Planning and Zoning Board previously recommended and the City Council authorized a variance so that the street improvements will not be required to be designed or constructed at this time, but delayed until further development is proposed. It is proposed that the same variance continue to apply to the amended configuration. Any looping of the water main would also be delayed and only required if modeling of the water system shows a need or substantial benefit for the proposed developments.



As two of the lots are planned to be sold before future development occurs, it is necessary to outline responsibility for the subdivision improvements. The applicant has provided a development agreement committing the lot owners to specific required subdivision improvements, tied to specific development actions. The language in the draft document, which is attached, has not been 100% finalized but is presented in its current form.

The development agreement addresses installation of the water main, sewer main, underground power, irrigation main, and the street connecting Sheridan Avenue with 37th Street as referenced in the master street plan and the initial review of this subdivision. Easements for the future utilities and the portion of the street that will cross Lot 1 are being established by the plat. Engineering plans for the sewer and water main extensions are being finalized—the prior plans are being updated to extend the mains to the new west boundary of Lot 2. The plans for the sewer and water mains will need to be submitted and then approved by the City and DEQ prior to the mayor signing the final plat.

In addition, there is an agreement between the applicant and the Cody Canal Irrigation District that is in process regarding the irrigation system, which agreement will need to be executed prior to the mayor signing the final plat. Additional paperwork beyond that agreement, relating to a water distribution plan (surface water), is being prepared by the applicant's engineer for submittal to Cody Canal and the State Engineer's office. As with other water right modifications, we are proposing that an agreement with an engineer be in place to complete the paperwork and submit it to the State Engineer's office within one year, but that the water distribution plan does not need to be approved by the State Engineer's office prior to recording the final plat.

Vacation of Unused Power Easement:

Included on the plat is the vacation of an unused power easement, established when the area was served by Rocky Mountain Power. As the Rocky Mountain Power easements in this area were transferred to the City and the easement is now unused due to a new underground power line in a different location, the unused easement is requested to be vacated. The vacation requires Council approval.

Street Improvement District.

As frontage improvements (curb, gutter, sidewalk, streetlights) are not being required along Beacon Hill Road or Sheridan Avenue, the following section of the subdivision code applies.

Q. Curb, Gutter, Sidewalk, Paved Streets: ...All waivers of curb, gutter and sidewalks shall require acknowledgment by the developer on the final plat that future improvement districts for the development of curb, gutter and sidewalks shall be supported by future owners of the lots and be so noted on the final plat. ...

With the waiver of frontage improvements along Beacon Hill Road and Sheridan Avenue, the standard improvement district language needs to be added to the "Certificate of Owner" on the final plat (applicable to Lots 1 and 2).

Minor Edits:

The following minor edits to the final plat are needed.

1. Include a note referencing that the lot owners are responsible for specific subdivision improvements as outlined in the Beacon Hill Minor Subdivision Development Agreement on file with the City of Cody.
2. Include a note that the owner of Lot 1 agrees to dedicate the 60' access easement as public right-of-way at no cost to the City upon the request of the City.
3. Add the improvement district language noted above.
4. Remove "East" from the Sheridan Avenue label.
5. Correct the legend and perimeter lot corners to show placement of brass caps, as required by City of Cody Code 11-5-1(B)(3).

VARIANCES:

Variances are to be reviewed under the following standard of 11-5-2(B):

If during the approval process of a proposed subdivision it can be shown that strict compliance with the requirements of this title will result in extraordinary hardship to the subdivider due to unusual topography or other similar land conditions, or where the subdivider can show that variances will make a greater contribution to the intent and purpose of this title, the commission and council may, upon written request and proper justification, grant a variance to this title so that substantial justice may be done and the public interest secured; provided, that any such variance will not have the effect of nullifying the intent and purpose of this title.

As with the initial review, the primary variance would be to delay submittal of street plans and street construction for the street connecting Sheridan Avenue and 37th Street until the time of future subdivision, or development of any lot utilizing the accesses.

A general variance to allow transfer of responsibility for some of the subdivision improvements from the developer to the lot purchasers, as outlined in the development agreement, would also be needed.

A variance would also be granted to the City's alley requirement, as no alleys are proposed at this time.

As with the initial review, staff would recommend that the Board find that the variances noted are found to "*make a greater contribution to the intent and purpose of this title*" than if those improvements were required at this time.

POTENTIAL MOTION:

Recommend that the City Council approve the amended Beacon Hill Minor Subdivision preliminary plat and the Beacon Hill Minor Subdivision final plat, with the following variances, subject to the conditions listed. The intent to vacate the unused electrical easement is also noted.

Subdivision Variances:

1. Variance to delay submittal of street plans and street construction for the street connecting Sheridan Avenue and 37th Street until the time of future subdivision, or development of any lot utilizing the accesses.
2. Variance to allow transfer of responsibility for some of the subdivision improvements from the developer to the lot purchasers, as outlined in the development agreement.
3. A variance to the alley requirement.

Conditions:

1. Provide updated sewer and water plans for City and DEQ approval. The plans must be approved by DEQ prior to the mayor signing the final plat.
2. Complete the agreement with Cody Canal regarding the irrigation improvements prior to the mayor signing the plat. Continue to have your engineer prepare and process the water distribution plan and associated water right documents for submittal to the State Engineer's office within one year of the final plat being recorded.
3. Pay the electrical estimate prior to the mayor signing the final plat.
4. Provide a letter from each telecommunication provider that will serve the lots indicating their requirements and ability to serve the subdivision, prior to the mayor signing the final plat. (Already have natural gas letter.)
5. Finalize and execute the Beacon Hill Minor Subdivision Development Agreement. Any modifications to the current draft will need to be identified and discussed with the City Council.
6. Make the following additions/changes to the final plat:
 - a. Add the standard improvement district language to the "Certificate of Owner". (Applicable to Lots 1 and 2. Optional for Lot 3.)
 - b. Include a note that the lot owners are responsible for specific subdivision improvements as outlined in the Beacon Hill Minor Subdivision Development Agreement on file with the City of Cody.
 - c. Include language in the "Certificate of Owner" that the owner of Lot 1 agrees to dedicate the 60' access easement as public right-of-way at no cost to the City upon the request of the City.
 - d. Remove "East" from the Sheridan Avenue label.
 - e. Correct the legend and perimeter lot corners to show placement of brass caps, as required by City of Cody Code 11-5-1(B)(3).

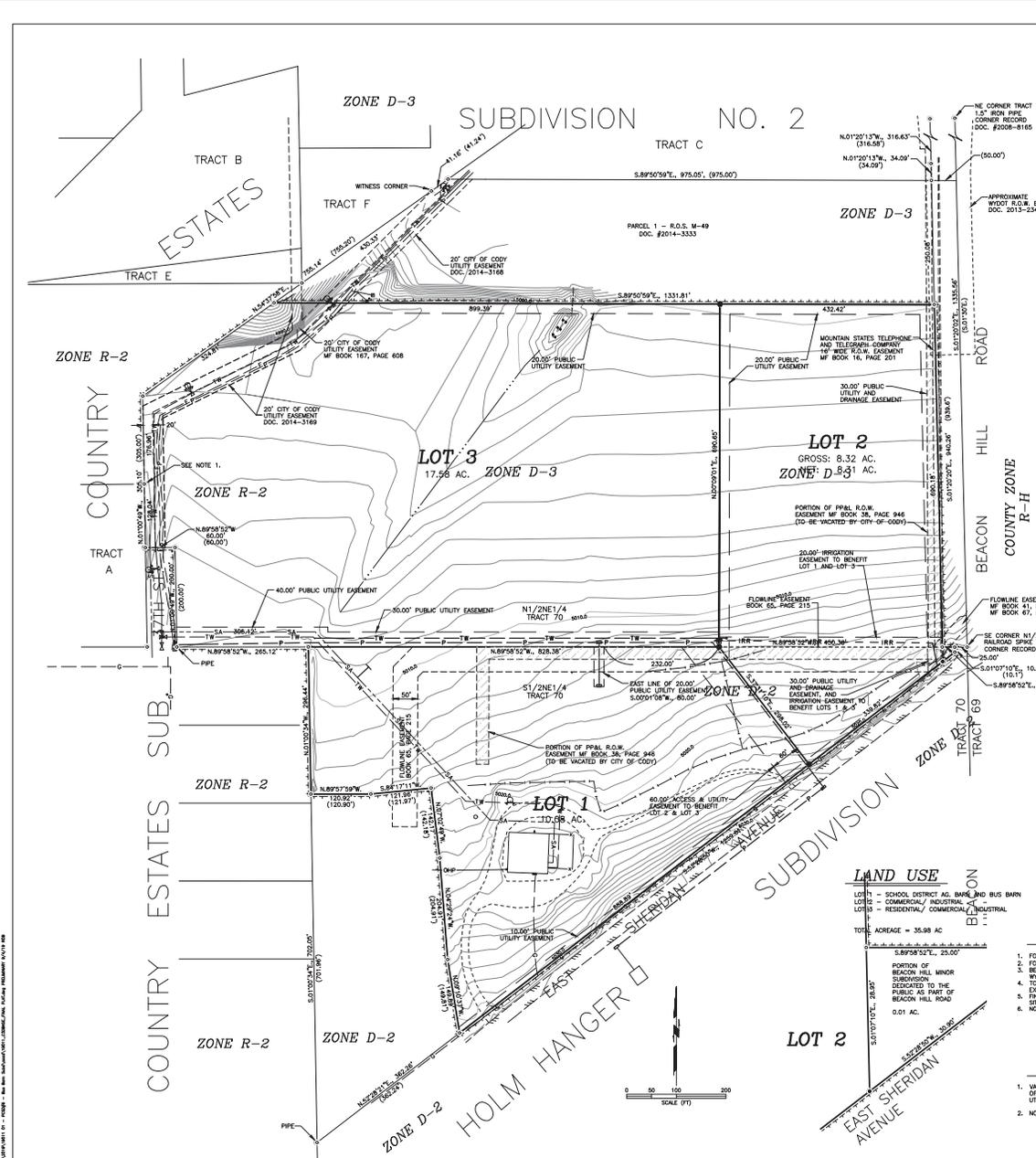
ATTACHMENTS:

Revised Preliminary Plat

Final Plat

Draft development agreement

(Available but not attached: Deeds and easement documents.)



CITY PLANNER APPROVAL

THIS BEACON HILL MINOR SUBDIVISION WAS APPROVED BY THE CITY PLANNER OF CODY, WYOMING ON THE ____ DAY OF _____ 20____.

BY: _____ ATTEST: _____
CITY PLANNER

CLERK AND RECORDER ACCEPTANCE

THIS BEACON HILL MINOR SUBDIVISION WAS ACCEPTED IN THE OFFICE OF THE CLERK AND RECORDER, PARK COUNTY, WYOMING, AT _____ O'CLOCK A.M. ON THIS ____ DAY OF _____ 20____.

FILED FOR RECORDING IN BOOK OR PLAT CABINET _____ AT PAGE _____ AND RECORDED AS COMPUTER RECORD DOCUMENT NUMBER _____.

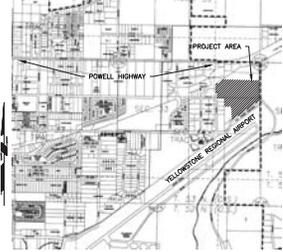
PARK COUNTY CLERK
BY: _____ DEPUTY.

CERTIFICATE OF SURVEYOR

I, LYLE J. CASADIA, OF ENGINEERING ASSOCIATES, HEREBY CERTIFY THAT THIS MAP WAS PREPARED USING DOCUMENTS ON FILE IN THE OFFICE OF THE PARK COUNTY CLERK, AND FROM NOTES TAKEN DURING ACTUAL SURVEYS PERFORMED UNDER MY DIRECTION BETWEEN SEPTEMBER 13, 2012 AND _____ 20____, AND THAT TO MY KNOWLEDGE, THIS MAP SHOWS THE SITUATION ON THE GROUND AT THE TIME OF ITS PREPARATION.

LYLE J. CASADIA, PLS 12800
06/14/19

**Township 53 North
Range 101 West**



LOCATION MAP
SCALE: 1" = 2000'

CERTIFICATE OF OWNER

STATE OF WYOMING } 25;
COUNTY OF PARK }
I, _____, OWNER OF THE FOREGOING CERTIFICATE OF OWNER WAS ACKNOWLEDGED BEFORE ME BY _____ THIS ____ DAY OF _____ 20____, WITNESS MY HAND AND OFFICIAL SEAL.
MY COMMISSION EXPIRES: _____

OWNER - PARK COUNTY SCHOOL DISTRICT NO. 6
BOARD CHAIR

NOTARY PUBLIC

LEGEND

- FOUND 2" DIA. ALUMINUM CAP ON 1/4" DIA. STEEL BAR UNLESS OTHERWISE NOTED.
- FOUND 2" DIA. ALUMINUM CAP ON 1/4" DIA. STEEL BAR.
- BEACON HILL MINOR SUBDIVISION BOUNDARY
- RIGHT-OF-WAY
- EXISTING EASEMENT
- EXISTING EASEMENT (15.5643707W, 366.25')
- PROPOSED BARRIED POWER
- PROPOSED BARRIED SEWER
- PROPOSED BARRIED GAS
- EXISTING BARRIED POWER
- EXISTING BARRIED GAS
- EXISTING BARRIED SEWER
- EXISTING BARRIED WATER
- EXISTING IRRIGATION PIPE
- EXISTING FIRE HOSEWAY
- EXISTING TREATED WATER SERVICE
- EXISTING POWER TRANSFORMER
- EXISTING FIBER OPTIC BOX
- EXISTING TELEPHONE PEDESTAL
- PROPOSED MANHOLE
- EXISTING MANHOLE

LAND USE

- LOT 1 - SCHOOL DISTRICT AC. BARN AND BUS BARN
 - LOT 2 - COMMERCIAL/INDUSTRIAL
 - LOT 3 - RESIDENTIAL/COMMERCIAL/INDUSTRIAL
- TOTAL ACRES = 35.96 AC

NOTE

1. FOUND 2" DIAMETER ALUMINUM CAP ON 5/8" DIA. STEEL BAR 0.17' NORTH OF LINE.
2. FOUND 2" DIAMETER ALUMINUM CAP ON 5/8" DIA. STEEL BAR 0.21' EAST OF LINE.
3. BEARINGS ARE BASED ON THE CITY OF CODY COORDINATE SYSTEM, WHICH IS BASED ON THE WYOMING COORDINATE SYSTEM NAD83 WEST CENTRAL ZONE.
4. TOTAL GROSS SUBDIVISION ACRES = 35.96 ACRES. TOTAL NET SUBDIVISION ACRES EXCLUDING STATE, COUNTY, AND PUBLIC RIGHTS-OF-WAY = 35.96 ACRES.
5. FINAL BARRIED POWER EXTENSION LOCATION WILL BE DETERMINED IN CONSTRUCTION WITH NEW BUS BARN SITE PLAN.
6. NO FLOOD AREAS OR WETLANDS OBSERVED ON SITE.

VARIANCES REQUESTED

1. VARIANCE TO DELAY SUBMITTAL OF STREET PLANS AND STREET CONSTRUCTION UNTIL THE TIME OF FUTURE SUBDIVISION OF LOTS 2 AND 3, OR UNTIL THE TIME OF DEVELOPMENT OF ANY LOT UTILIZING THE 37TH STREET ACCESS OR EASEMENT.
2. NO ALLEYS ARE REQUIRED FOR THIS SUBDIVISION.

**PRELIMINARY PLAT-REV.
BEACON HILL MINOR
SUBDIVISION**

LOCATED IN
LOT 15A, HOLM HANGER SUBDIVISION, CITY OF CODY,
RESURVEY T.53N., R.101W., 6TH P.M.,
PARK COUNTY, WYOMING

PREPARED FOR: PARK COUNTY SCHOOL DISTRICT NO. 6
919 CODY AVENUE
CODY, WYOMING 82414

PREPARED BY: ENGINEERING ASSOCIATES
CONSULTING ENGINEERS & SURVEYORS
902 13TH STREET
CODY, WYOMING 82414



Township 53 North
Range 101 West



LOCATION MAP
SCALE: 1" = 2000'

CITY PLANNER APPROVAL

THE BEACON HILL MINOR SUBDIVISION WAS APPROVED BY THE CITY PLANNER OF CODY, WYOMING ON THE ____ DAY OF _____, 2019.

BY: _____ CITY PLANNER ATTEST: _____

CITY COUNCIL APPROVAL

APPROVED AS OF ____ DAY OF _____, 2019 BY THE CITY COUNCIL OF CODY, WYOMING.

MAYOR - MATT HALL

ATTEST: CINDY BAKER ADMINISTRATIVE SERVICES OFFICER

CLERK AND RECORDER ACCEPTANCE

THIS BEACON HILL MINOR SUBDIVISION WAS ACCEPTED IN THE OFFICE OF THE CLERK AND RECORDER, PARK COUNTY, WYOMING, ON ____ DAY OF _____, 2019.

FILED FOR RECORDING IN BOOK OR PLAT CABINET ____ AT PAGE ____ AND RECORDED AS COMPUTER RECORD DOCUMENT NUMBER ____

PARK COUNTY CLERK
BY: _____ DEPUTY.

CERTIFICATE OF SURVEYOR

I, LYLE J. CASCIATO, OF ENGINEERING ASSOCIATES, HEREBY CERTIFY THAT THIS MAP WAS PREPARED USING DOCUMENTS ON FILE IN THE OFFICE OF THE PARK COUNTY CLERK AND FROM NOTES TAKEN DURING ACTUAL SURVEYING PERFORMED UNDER MY SUPERVISION BETWEEN SEPTEMBER 13, 2012 AND SEPTEMBER 20, 2019, AND THAT I DO KNOW THE MAP SHOWS THE "STRENGTH" ON THE GROUND AT THE TIME OF ITS PREPARATION.

DRAFT

LYLE J. CASCIATO, PLS 12600
08/08/2019

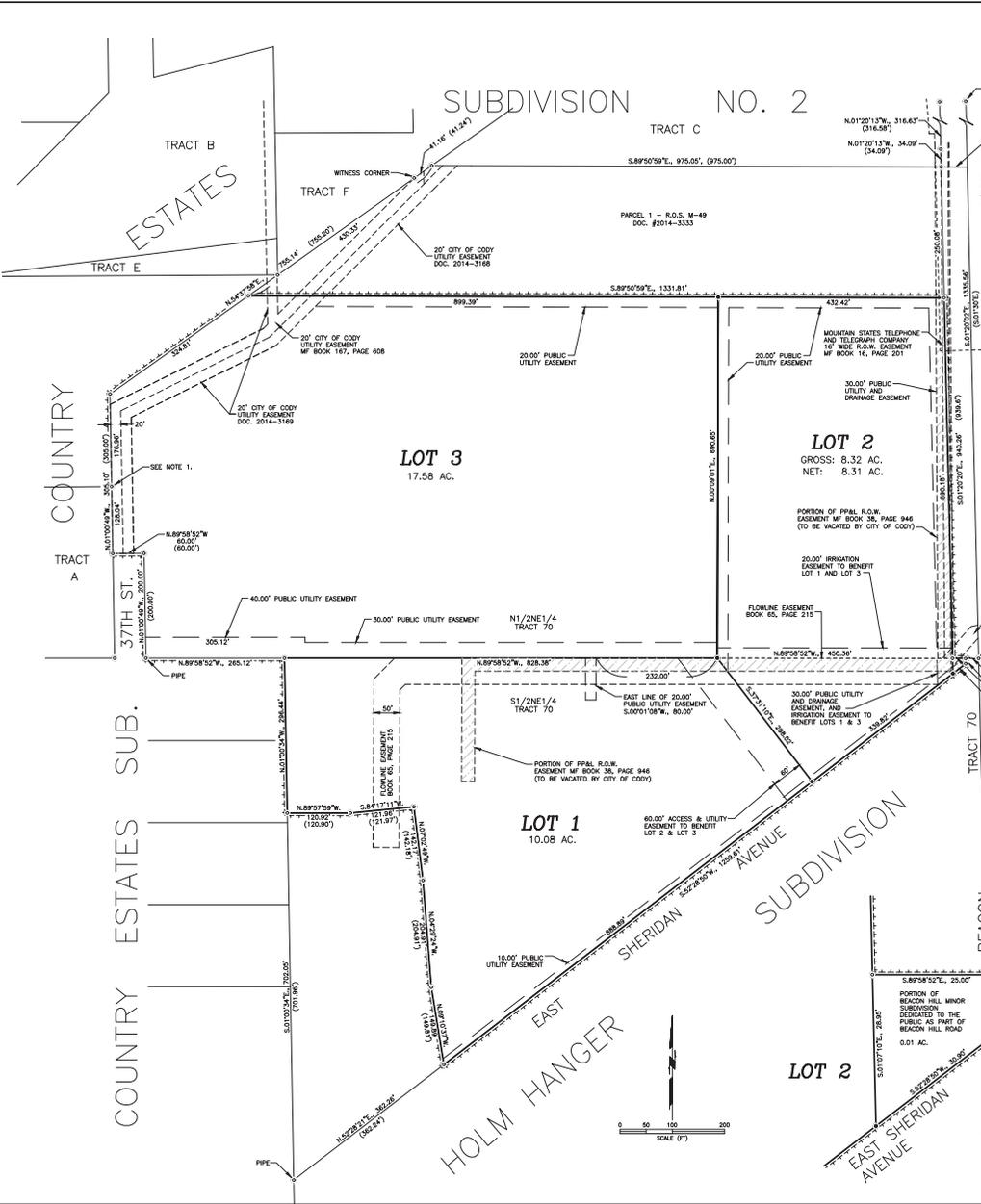
LEGEND

- FOUND 2" DIA. ALUMINUM CAP ON 5/8" DIA. STEEL BAR UNLESS OTHERWISE NOTED
- SET 2" DIA. ALUMINUM CAP ON 5/8" DIA. STEEL BAR
- BEACON HILL MINOR SUBDIVISION BOUNDARY EASEMENT CREATED BY THIS PLAT
- - - - - EXISTING EASEMENT
- (---) 0.566470700'±, 365.22' SECOND SURVEY DIMENSIONS
- (---) 0.566470700'±, 365.22' MEASURED DIMENSION, THE SURVEY
- (---) 0.01 AC. PORTION OF BEACON HILL MINOR SUBDIVISION DEDICATED TO THE PUBLIC AS PART OF BEACON HILL ROAD

NOTES

1. FOUND 2" DIAMETER ALUMINUM CAP ON 5/8" DIA. STEEL BAR 3.21' EAST OF LINE.
2. BEARINGS ARE BASED ON THE CITY OF CODY COORDINATE SYSTEM, WHICH IS BASED ON THE WYOMING COORDINATE SYSTEM NAD83 WEST CENTRAL ZONE.
3. TOTAL GROSS SUBDIVISION ACREAGE = 35.98 ACRES. TOTAL NET SUBDIVISION ACREAGE EXCLUDING STATE, COUNTY, AND PUBLIC RIGHTS-OF-WAY = 33.97 ACRES.

DETAIL



CERTIFICATE OF OWNER

STATE OF WYOMING }
COUNTY OF PARK } SS.

I, _____, OF THE COUNTY OF PARK, STATE OF WYOMING, HEREBY CERTIFY THAT WE ARE OWNERS AND PROPRIETORS OF THE LANDS SHOWN HEREON, CONSISTING OF LOT 15A, HOLM HANGER SUBDIVISION, AS DEPICTED ON THAT RECORD OF SURVEY RECORDED AS DOCUMENT #2014-3333 AND FILED IN PLAT CABINET M AT PAGE 49 IN THE OFFICE OF THE PARK COUNTY CLERK.

THAT WE HAVE CAUSED SAID LANDS TO BE SURVEYED AND PLATTED AS SHOWN HEREON AS BEACON HILL MINOR SUBDIVISION LOCATED WITHIN THE CITY OF CODY, WYOMING. THAT THIS SUBDIVISION OF SAID LANDS IS WITH THE FREE CONSENT AND IN ACCORDANCE WITH THE DESIRES OF THE UNDERSIGNED OWNERS AND PROPRIETORS; THAT LOT 15A IS SUBJECT TO ALL COVENANTS, TERMS AND CONDITIONS OF LOT 14 OF THE HOLM HANGER SUBDIVISION, THAT LOTS 1, 2 AND 3 OF THE BEACON HILL MINOR SUBDIVISION ARE SUBJECT TO ALL COVENANTS, TERMS AND CONDITIONS OF LOT 15 OF THE HOLM HANGER SUBDIVISION THAT WE HEREBY DEDICATE TO THE PUBLIC THAT PORTION OF LAND DESIGNATED AS BEACON HILL ROAD AS SHOWN HEREON. ALL NEW UTILITY EASEMENTS LABELED HEREON TO THE USE OF PUBLIC UTILITIES AND DEDICATE ALL OTHER EASEMENTS TO THE USES SO NOTED. THAT SAID LANDS ARE SUBJECT TO ANY EASEMENTS, RIGHTS-OF-WAY AND MINERAL RIGHTS OR RESERVATIONS ON RECORD. ANY RIGHTS BY VIRTUE OF THE HOMESTEAD EXEMPTION LAWS OF THE STATE OF WYOMING ARE HEREBY RELEASED AND WAIVED.

OWNER - PARK COUNTY SCHOOL DISTRICT NO. 6: _____ BOARD CHAIR
STATE OF WYOMING } SS.
COUNTY OF PARK } SS.

THE FOREGOING CERTIFICATE OF OWNER WAS ACKNOWLEDGED BEFORE ME BY _____ THIS ____ DAY OF _____, 20____, WITNESS MY HAND AND OFFICIAL SEAL.

MY COMMISSION EXPIRES: _____

CERTIFICATE OF VACATION

STATE OF WYOMING } SS.
COUNTY OF PARK } SS.

I, _____, OF THE COUNTY OF PARK, STATE OF WYOMING, HEREBY CERTIFY THAT WE ARE THE BENEFICIARIES OF THAT RIGHT-OF-WAY EASEMENT 25 FEET IN WIDTH FOR AN ELECTRIC TRANSMISSION AND DISTRIBUTION LINE AS RECORDED IN MEMORIAL ENTRY BOOK 38 AT PAGE 848 IN THE OFFICE OF THE PARK COUNTY CLERK. THAT THE PORTION OF THIS RIGHT-OF-WAY EASEMENT THAT CROSSES THE BEACON HILL MINOR SUBDIVISION IS NO LONGER USED FOR ITS STATED PURPOSE, AND THAT WE HEREBY WAIVE AND EXTINGUISH ALL OF THIS RIGHT-OF-WAY EASEMENT WHICH LIES WITHIN BEACON HILL MINOR SUBDIVISION.

MAYOR - MATT HALL
ATTEST: CINDY BAKER ADMINISTRATIVE SERVICES OFFICER

**PLAT OF
BEACON HILL MINOR
SUBDIVISION**

LOCATED IN
LOT 15A, HOLM HANGER SUBDIVISION, RECORD OF SURVEY FILED IN PLAT CABINET M AT PAGE 49, ALSO BEING WITHIN THE N1/2NE1/4 OF TRACT 70 AND PART OF LOT 15, HOLM HANGER SUBDIVISION, CITY OF CODY, RESURVEY T.53N., R.101W., 6TH P.M., PARK COUNTY, WYOMING

PREPARED FOR: PARK COUNTY SCHOOL DISTRICT NO. 6
919 CODY AVENUE
CODY, WYOMING 82414

PREPARED BY: ENGINEERING ASSOCIATES
CONSULTING ENGINEERS & SURVEYORS
902 13TH STREET
CODY, WYOMING 82414



Beacon Hill Minor Subdivision

Development Agreement

Park County School District No. 6 hereby presents the following commitment to the City of Cody, which in consideration of approval of the final plat of the Beacon Hill Minor Subdivision shall be binding on Park County School District No. 6 and the purchasers of any lot within the Beacon Hill Minor Subdivision.

1. Purchaser of Lot 2 will be required to install the sewer main line from the NW corner of Lot 3 to the NW corner of Lot 2, as specified in the engineering plans approved by DEQ and on file with the City, prior to or in conjunction with any development of Lot 2, or upon approval of any development of Lot 3 that will utilize that sewer main line, whichever occurs first.
2. Purchaser of Lot 2 will be required to complete the installation of an 8-inch city water line from the bend near the NW corner of Lot 1 to Lot 2, as specified in the engineering plans approved by DEQ and on file with the City, prior to or in conjunction with any development of Lot 2, or upon approval of any development of Lot 1 that will utilize that water main line, whichever occurs first.
3. Three phase power for Lot 2 will be available at the SW corner of Lot 2. PCSD#6 will be responsible for extending underground power to the SW corner of Lot 2 upon approval of any development that will utilize that extension. The city electrical division has required that this power distribution be looped by extending the underground power line to the proposed electrical line at the planned Shoshone Municipal Pipeline building across Sheridan Avenue from Lot 1. PCSD#6 is responsible for installation of the power loop at the time the City installs power to the planned Shoshone Municipal Pipeline Pump Station, but not prior to approval of any development on property within the Beacon Hill minor subdivision.
4. Pursuant to Cody Canal regulations, future irrigation must be through a sprinkler system. Flood irrigation within the subdivision shall cease once either Lot 2 or Lot 3 sells. The owners of Lots 2 and 3 will be responsible for installation of piped irrigation water delivery from the head-gate located at the NE corner of the airport property to the intersection of Lots 1, 2, and 3. The cost sharing will be as follows: Lot 2 @ 50%, Lot 3 @ 50%. Piping will be installed at the earliest request of either Lot 2 or Lot 3 owners. The design will be approved by Cody Canal and the State Engineer's office prior to construction.

5. Lot 3 is responsible to construct a public street from the intersection of 37th Street and Cherokee Road, through the subdivision, to Sheridan Avenue. Street design shall include storm water retention for runoff from the street and otherwise meet City standards, as verified by a licensed engineer and submittal of all documents pursuant to section 11-5-1(A) of the subdivision ordinance. Any right-of-way needed for construction of the street improvements across Lot 1 or 3 shall be dedicated to the City upon request.

As the size and design of the street is dependent upon the layout and level of development planned for Lot 3, the City has allowed for a delay in submittal of the street plans and street construction until the time Lot 3 is further subdivided, or until the development of any existing lot utilizing the street. The specific timing of street construction, including any phasing, shall be determined through the review process for the future development (subdivision or zoning review). The street improvements shall be based on the local access street standard, unless determined otherwise by the City Council.

6. The Lot 3 purchaser is responsible for any modifications to the existing drainage facilities as a result of the street construction.

(Signed, dated and notarized by Park County School District No. 6. Add notary block.)

**CITY OF CODY
PLANNING, ZONING AND ADJUSTMENT BOARD
STAFF REPORT**

MEETING DATE:	SEPTEMBER 10, 2019	TYPE OF ACTION NEEDED	
AGENDA ITEM:		P&Z BOARD APPROVAL:	X
SUBJECT:	DOWNTOWN ARCHITECTURAL DISTRICT REVIEW: CODY COUNTRY ART LEAGUE SIGN. SGN 2018-30	RECOMMENDATION TO COUNCIL:	
PREPARED BY:	TODD STOWELL	DISCUSSION ONLY:	

PROJECT DESCRIPTION:

On November 13, 2018 the Board approved a 2-foot by 8-foot wall sign on the west side of the building at 836 Sheridan Avenue for the Cody Country Art League.

The Art League would like to add a sign indicating "Art Gallery/Gift Shop" immediately below the approved sign. The new sign would be half the height of the existing sign. While it would have the same font, it would switch the background color to red and the lettering to light yellow—opposite of the existing.



The amended proposal meets the size and location requirements for the downtown sign district in which it is located.

RECOMMENDATION:

Approve the additional wall sign for the Cody Country Art League as proposed.